



To York Plains →

56805.670 CT
 56800

56900

57000

For Continuation Refer Sheet 19

For Continuation Refer Sheet 21

Control Line MC01

← To Jericho

Legend

- DP Property boundary
- Existing Fence line
- Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
- Pavement Type 3 - Road widening
- Pavement Type 5 - Road widening
- Pavement Type 6 - Road widening
- Pavement Type 7 - Jordan River Bridge Approach Pavement
- Pavement Type 8 - Bridge Deck Surfacing
- Local Property Access Pavement
- Pavement Rejuvenation
- Existing Seal

Notes

1. Refer to sheet 4551 and 4552 for pavement details.
2. Widening locations are indicative only, and are subject to minimum construction widths and note 10 of sheet 4552.
3. For Local Property Access Pavement, the seal extent is to the property boundary or access gate.
4. Decommission any existing residual pavement in accordance with D.S.G. Specification Section 160.22

DESIGN

Warning
 Beware of Underground Services
 The location of underground services are approximate only and the exact position should be proven on site. No guarantee is given that all services are shown on these drawings.

19/11/2021 11:52:39 AM

19/11/2021

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

Out of Scope

DESIGNED _____
 REVIEWED _____

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 20

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4620	PRINTED DATE 19/11/2021	SHEET No. 4620
REGISTRATION NUMBER A0087.059			REVISION 0

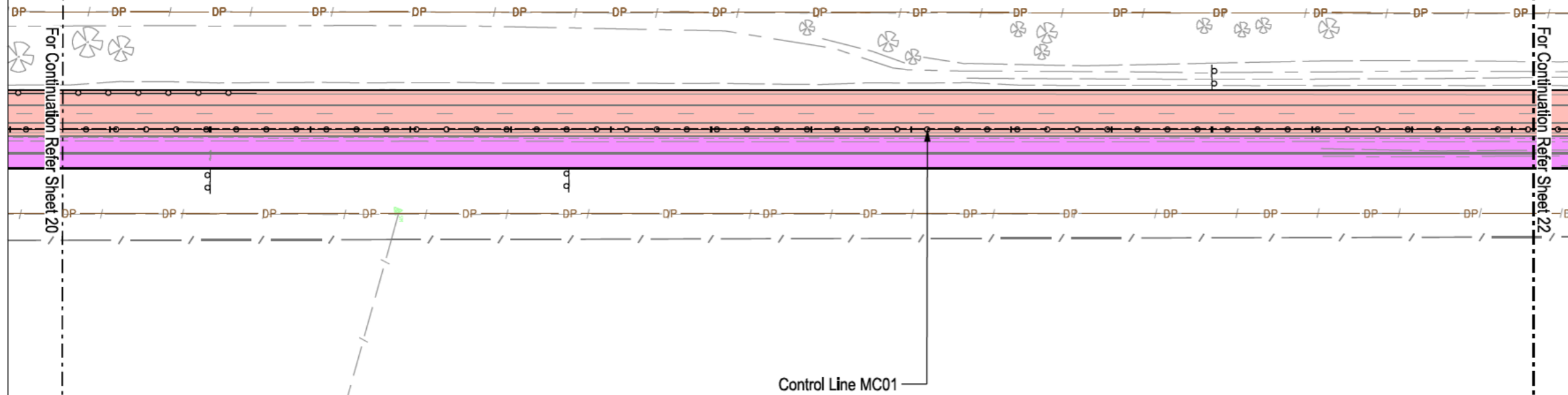


To York Plains →

57100

57200

57300



Legend

- Property boundary
- Existing Fence line
- Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
- Pavement Type 3 - Road widening
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DESIGN

19/11/2021 11:52:58 AM

19/11/2021

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21
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SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

Tasmanian Government

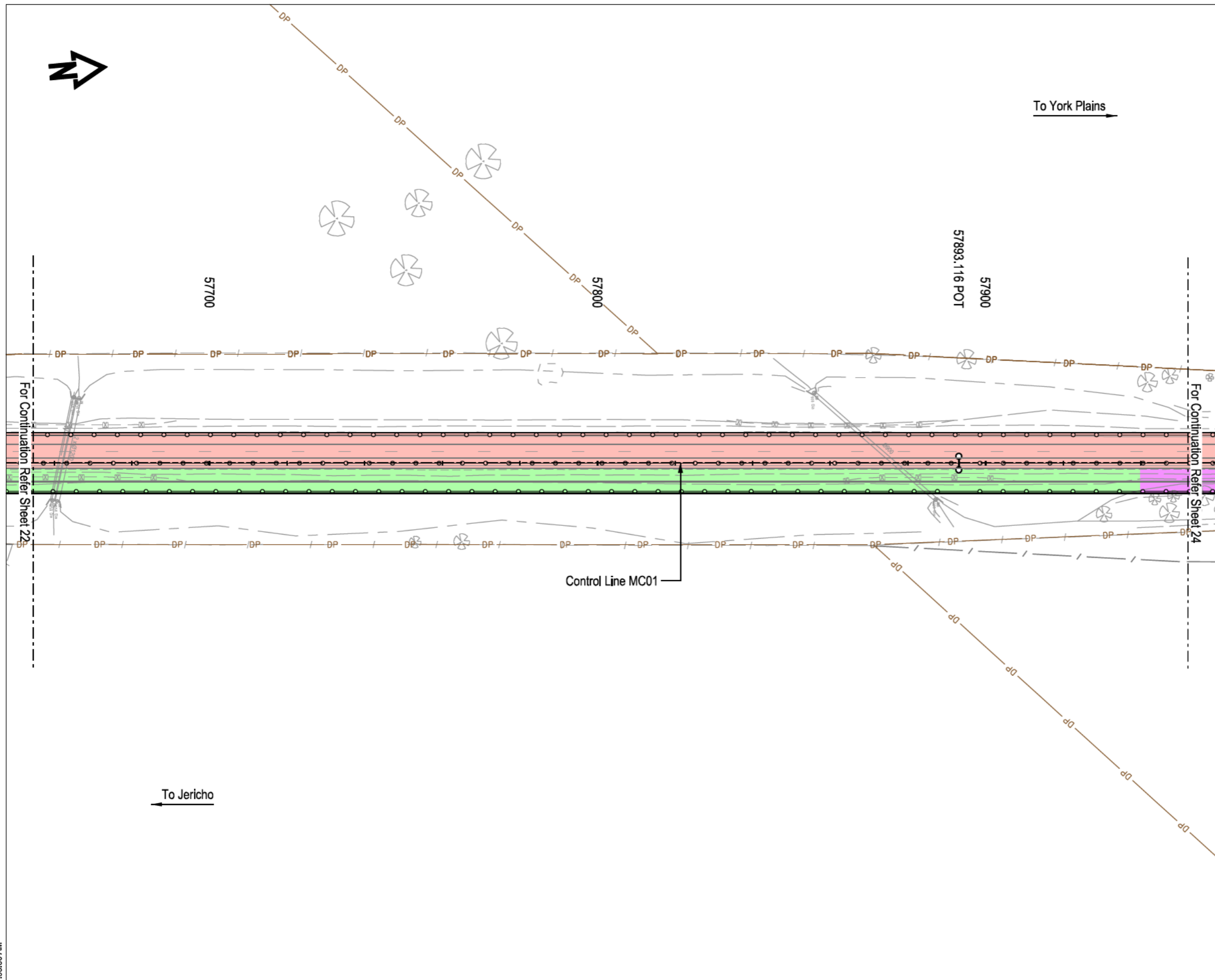
DESIGNED Out of Scope

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 21

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4621	PRINTED DATE 19/11/2021	SHEET No. 4621
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

- DP Property boundary
- Existing Fence line
- Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
- Pavement Type 3 - Road widening
- Pavement Type 5 - Road widening
- Pavement Type 6 - Road widening
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- Pavement Type 8 - Bridge Deck Surfacing
- Local Property Access Pavement
- Pavement Rejuvenation
- Existing Seal

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DESIGN

19/11/2021 11:53:30 AM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

DESIGNED Out of Scope

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 23

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4623	PRINTED DATE 19/11/2021	SHEET No. 4623
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains →

58000

58100

58200

Control Line MC01

← To Jericho

Legend

-  DP Property boundary
-  Existing Fence line
-  Proposed fence line
-  Pavement Type 1a - Road widening
-  Pavement Type 1b - Road widening
-  Pavement Type 2a - Road widening
-  Pavement Type 2b - Road widening
-  Pavement Type 3 - Road widening
-  Pavement Type 5 - Road widening
-  Pavement Type 6 - Road widening
-  Pavement Type 7 - Jordan River Bridge Approach Pavement
-  Pavement Type 8 - Bridge Deck Surfacing
-  Local Property Access Pavement
-  Pavement Rejuvenation
-  Existing Seal

Notes

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DESIGN


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19/11/2021 11:53:45 AM

19/11/2021

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES



SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs 

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
PAVEMENT EXTENTS PLAN
SHEET 24

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4624	PRINTED DATE 19/11/2021	SHEET No. 4624
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains →

← To Jericho

Legend

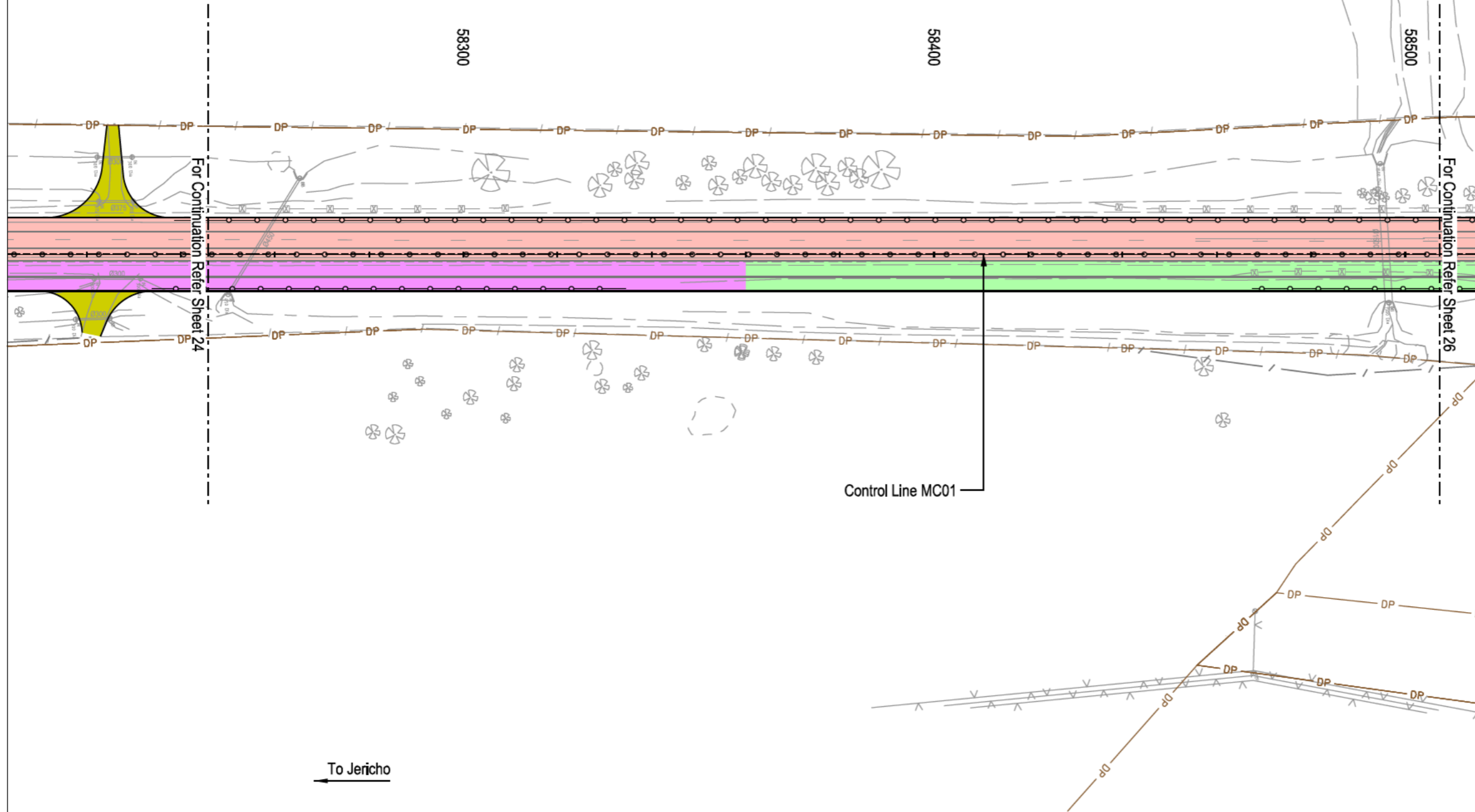
-  DP Property boundary
-  Existing Fence line
-  Proposed fence line
-  Pavement Type 1a - Road widening
-  Pavement Type 1b - Road widening
-  Pavement Type 2a - Road widening
-  Pavement Type 2b - Road widening
-  Pavement Type 3 - Road widening
-  Pavement Type 5 - Road widening
-  Pavement Type 6 - Road widening
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-  Pavement Type 8 - Bridge Deck Surfacing
-  Local Property Access Pavement
-  Pavement Rejuvenation
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DESIGN




19/11/2021 11:54:03 AM

19/11/2021

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES



SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs



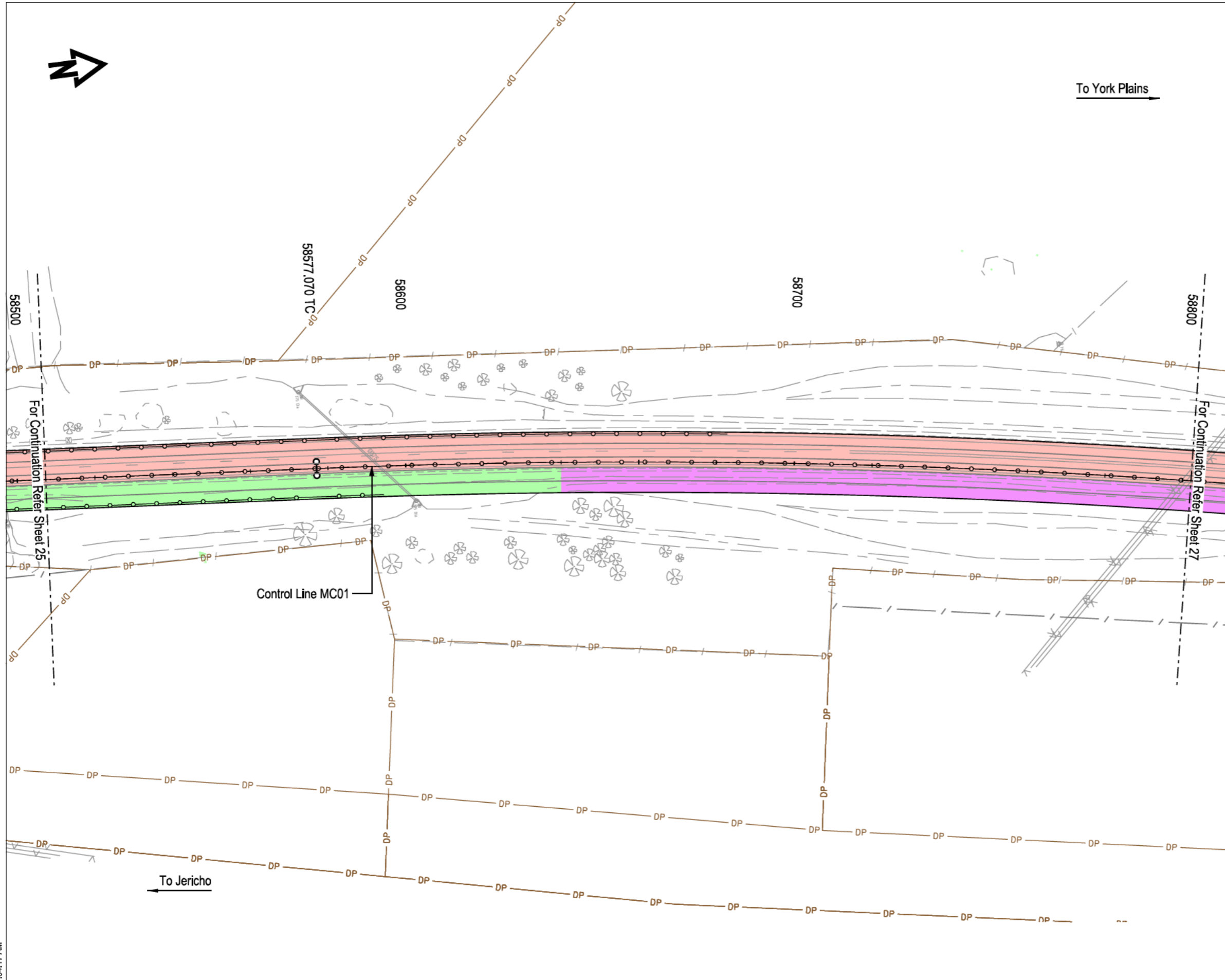
Out of Scope

DESIGNED
REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
PAVEMENT EXTENTS PLAN
SHEET 25

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4625	PRINTED DATE 19/11/2021	SHEET No. 4625
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

- DP — Property boundary
- - - Existing Fence line
- / - Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
- Pavement Type 3 - Road widening
- Pavement Type 5 - Road widening
- Pavement Type 6 - Road widening
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- Pavement Type 8 - Bridge Deck Surfacing
- Local Property Access Pavement
- Pavement Rejuvenation
- Existing Seal

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No.	Amendment Description	Initials	Date
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SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**

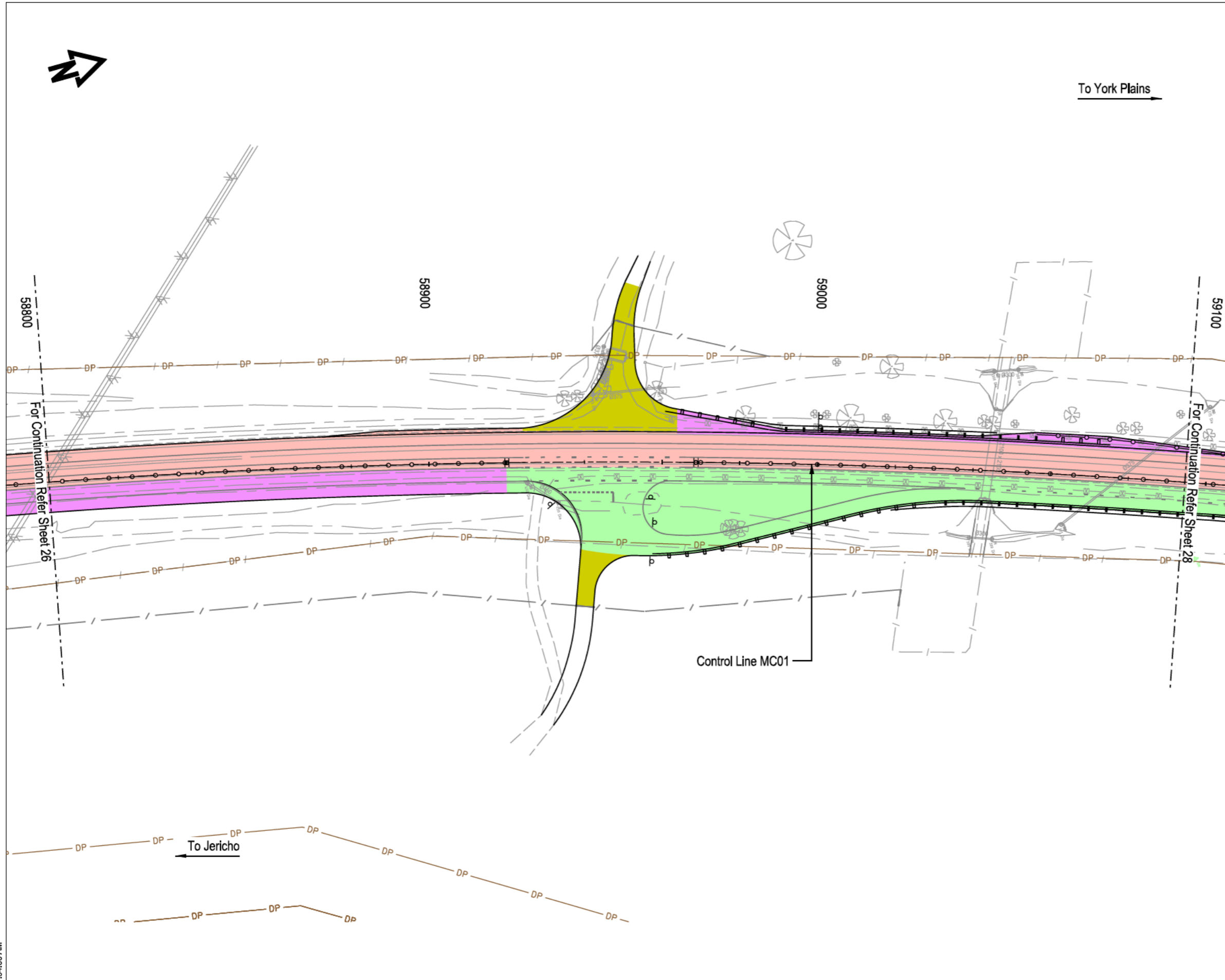
REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 26

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4626	PRINTED DATE 19/11/2021	SHEET No. 4626
REGISTRATION NUMBER A0087.059			REVISION 0

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Legend

- DP Property boundary
- Existing Fence line
- Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
- Pavement Type 3 - Road widening
- Pavement Type 5 - Road widening
- Pavement Type 6 - Road widening
- Pavement Type 7 - Jordan River Bridge Approach Pavement
- Pavement Type 8 - Bridge Deck Surfacing
- Local Property Access Pavement
- Pavement Rejuvenation
- Existing Seal

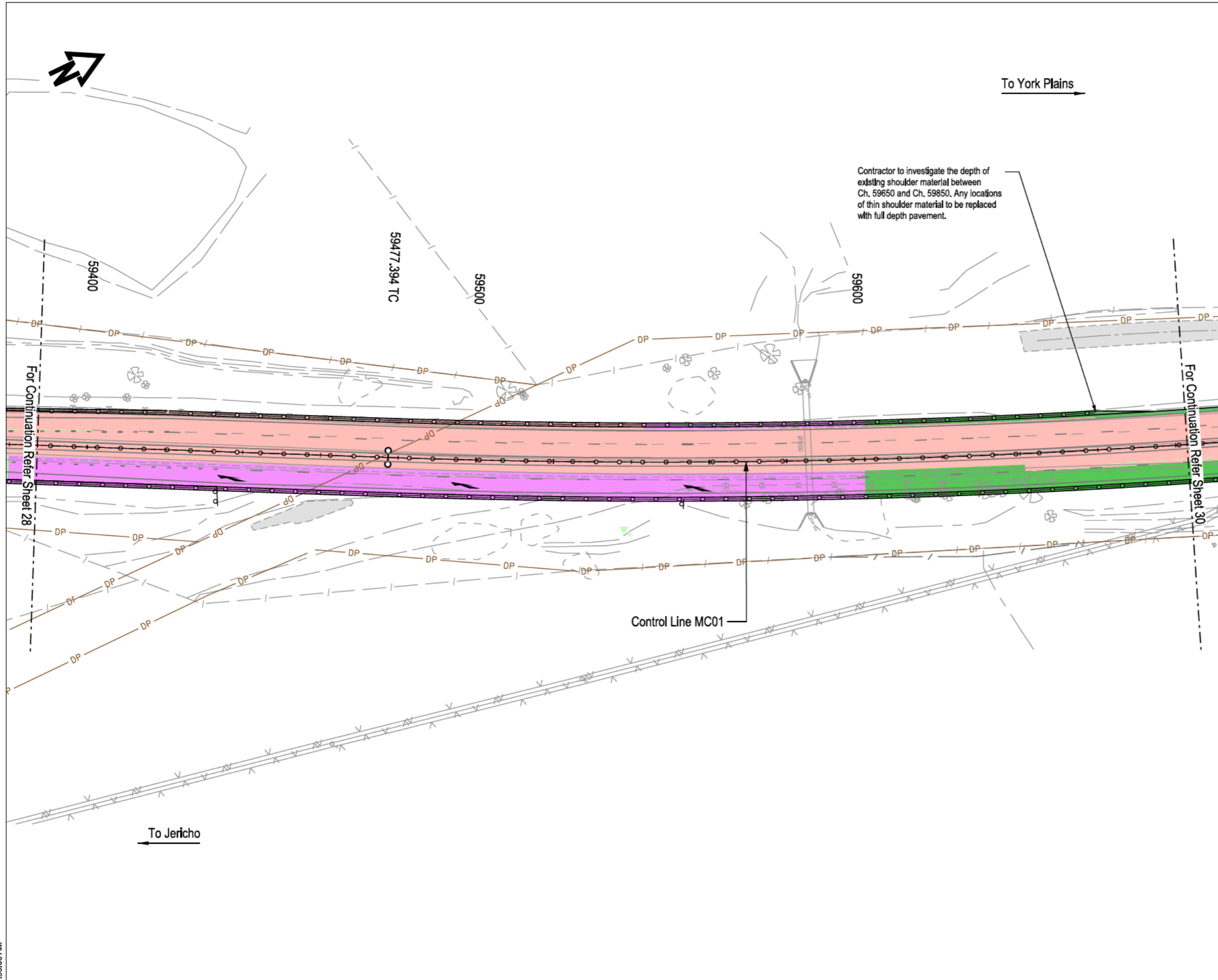
- Notes**
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DESIGN

	SCALES SCALE 1:1000 (A3)	 Tasmanian Government	Department of State Growth MIDLAND HIGHWAY (A0087) JERICO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE PAVEMENT EXTENTS PLAN SHEET 27	CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4627	PRINTED DATE 19/11/2021	SHEET No. 4627
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19/11/2021 11:54:33 AM



Legend

- DP Property boundary
- - - Existing Fence line
- / - Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
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- Pavement Type 6 - Road widening
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- Pavement Type 8 - Bridge Deck Surfacing
- Local Property Access Pavement
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DESIGN

19/11/2021 11:55:03 AM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

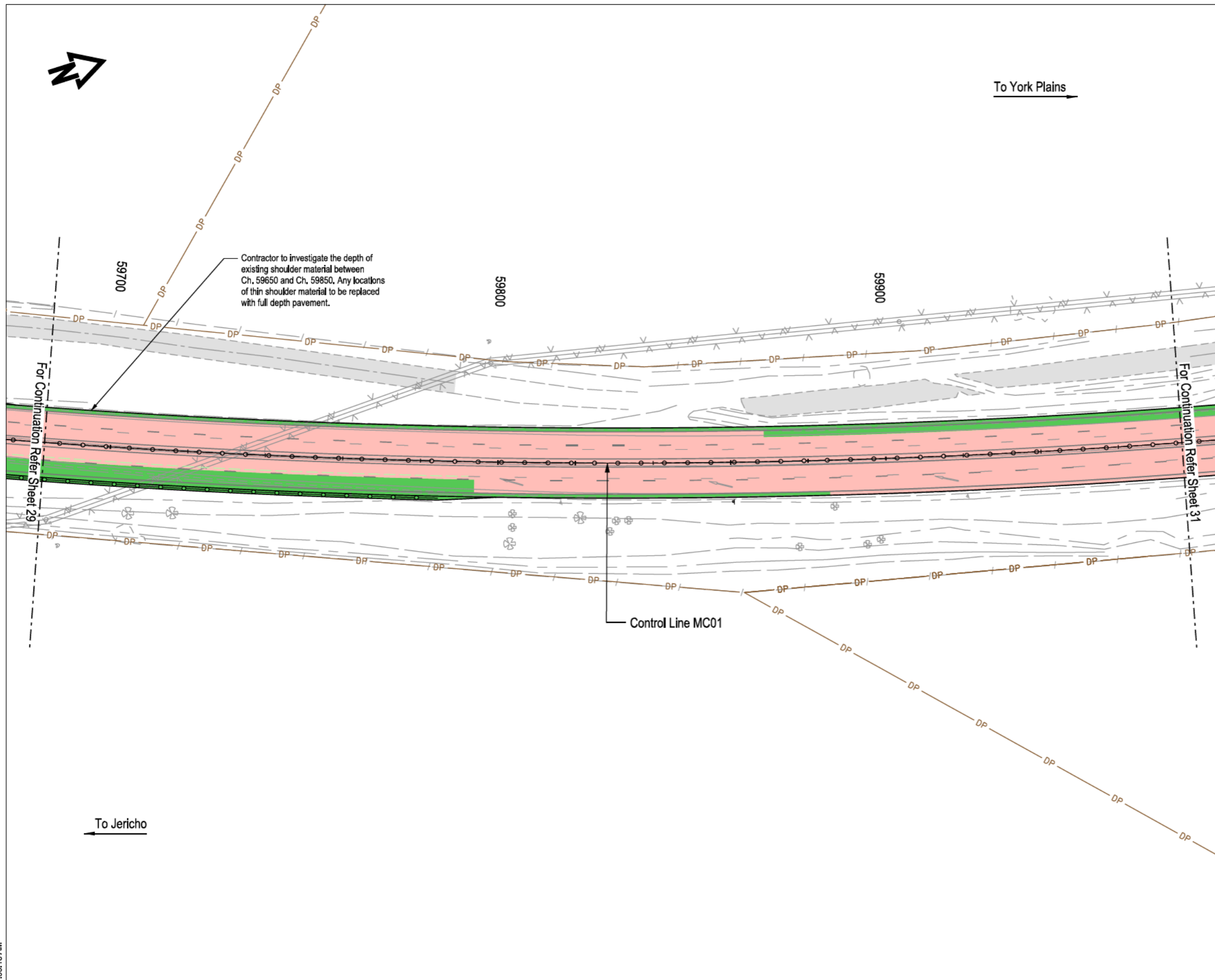
Out of Scope

DESIGNED
REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 29

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4629	PRINTED DATE 19/11/2021	SHEET No. 4629
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

- Property boundary
- Existing Fence line
- Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
- Pavement Type 2a - Road widening
- Pavement Type 2b - Road widening
- Pavement Type 3 - Road widening
- Pavement Type 5 - Road widening
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- Local Property Access Pavement
- Pavement Rejuvenation
- Existing Seal

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DESIGN

19/11/2021 11:58:18 AM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

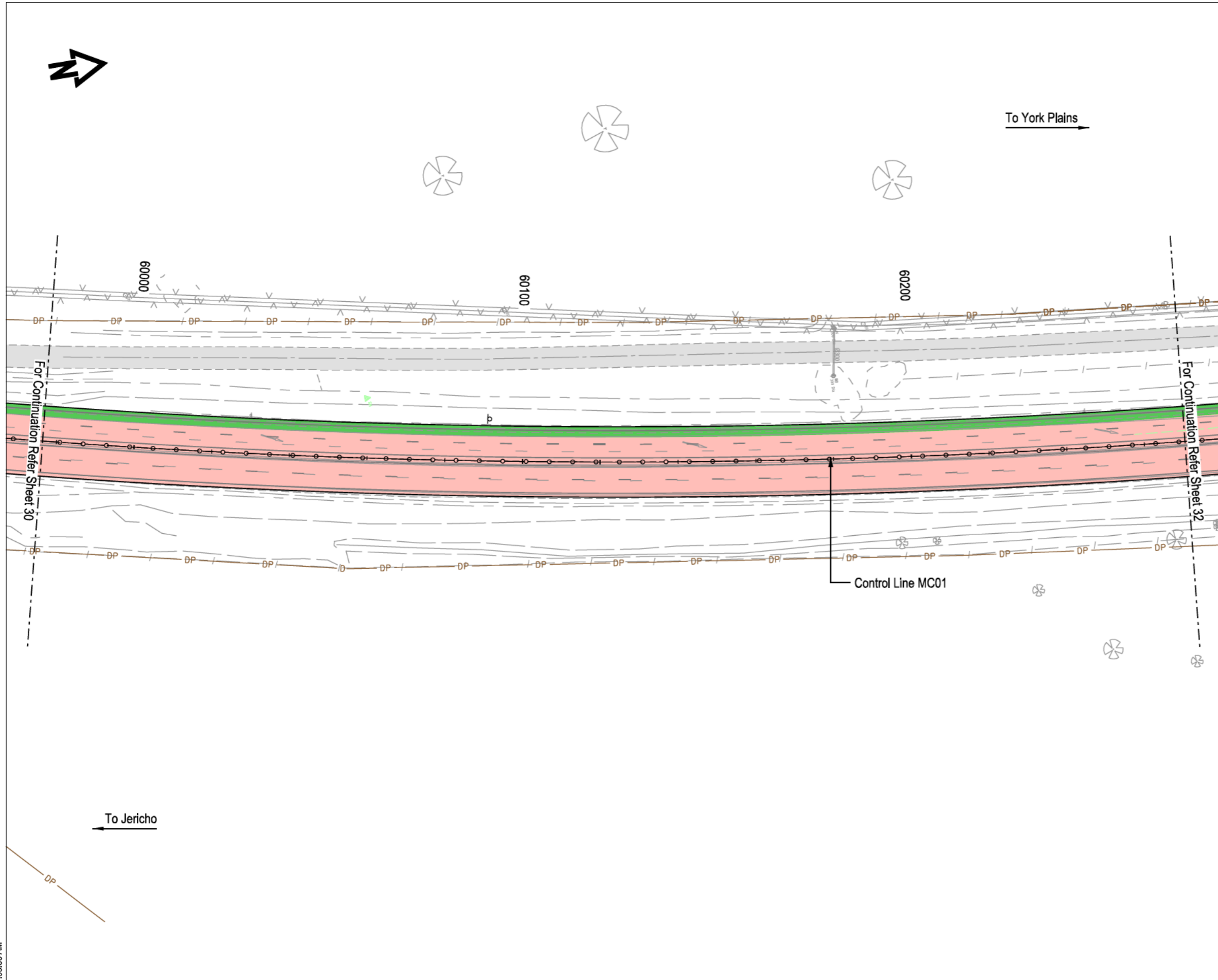
DESIGNED **Out of Scope**

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Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 30

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4630	PRINTED DATE 19/11/2021	SHEET No. 4630
REGISTRATION NUMBER A0087.059			REVISION 0



- Legend**
- DP — Property boundary
 - - - Existing Fence line
 - - - Proposed fence line
 - Pavement Type 1a - Road widening
 - Pavement Type 1b - Road widening
 - Pavement Type 2a - Road widening
 - Pavement Type 2b - Road widening
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DESIGN

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No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21
A3 original	This sheet may be prepared using colour and may be incomplete if copied		

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

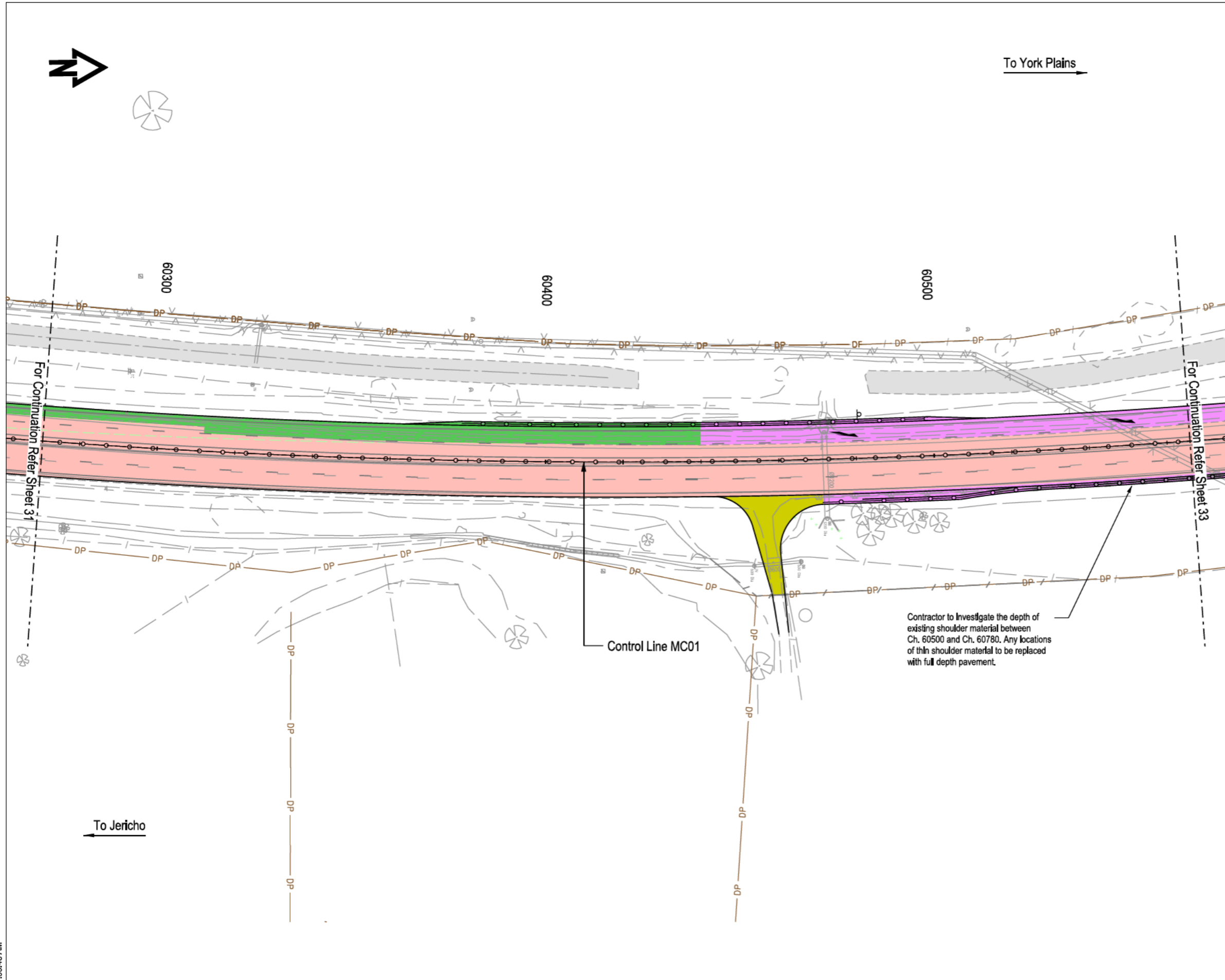
Out of Scope

DESIGNED
REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 31

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4631	PRINTED DATE 19/11/2021	SHEET No. <b style="font-size: 1.5em;">4631
REGISTRATION NUMBER <b style="font-size: 1.2em;">A0087.059			REVISION 0



- Legend**
- Property boundary
 - Existing Fence line
 - Proposed fence line
 - Pavement Type 1a - Road widening
 - Pavement Type 1b - Road widening
 - Pavement Type 2a - Road widening
 - Pavement Type 2b - Road widening
 - Pavement Type 3 - Road widening
 - Pavement Type 5 - Road widening
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Contractor to investigate the depth of existing shoulder material between Ch. 60500 and Ch. 60780. Any locations of thin shoulder material to be replaced with full depth pavement.

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DESIGN

19/11/2021 11:55:48 AM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

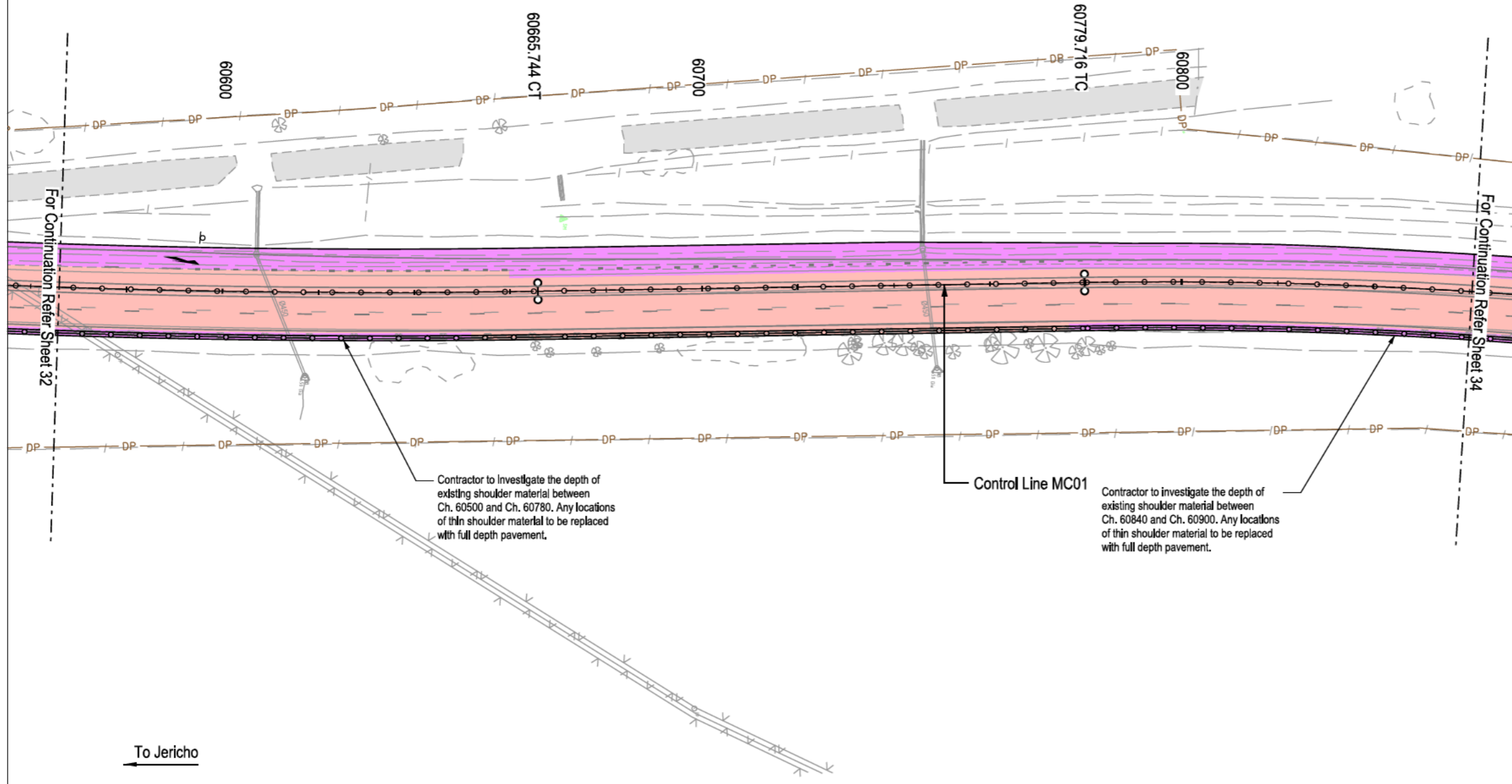
MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
PAVEMENT EXTENTS PLAN
SHEET 32

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4632	PRINTED DATE 19/11/2021	SHEET No. 4632
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains

To Jericho



Contractor to investigate the depth of existing shoulder material between Ch. 60500 and Ch. 60780. Any locations of thin shoulder material to be replaced with full depth pavement.

Control Line MC01
Contractor to investigate the depth of existing shoulder material between Ch. 60840 and Ch. 60900. Any locations of thin shoulder material to be replaced with full depth pavement.

Legend

- Property boundary
- Existing Fence line
- Proposed fence line
- Pavement Type 1a - Road widening
- Pavement Type 1b - Road widening
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DESIGN

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SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**

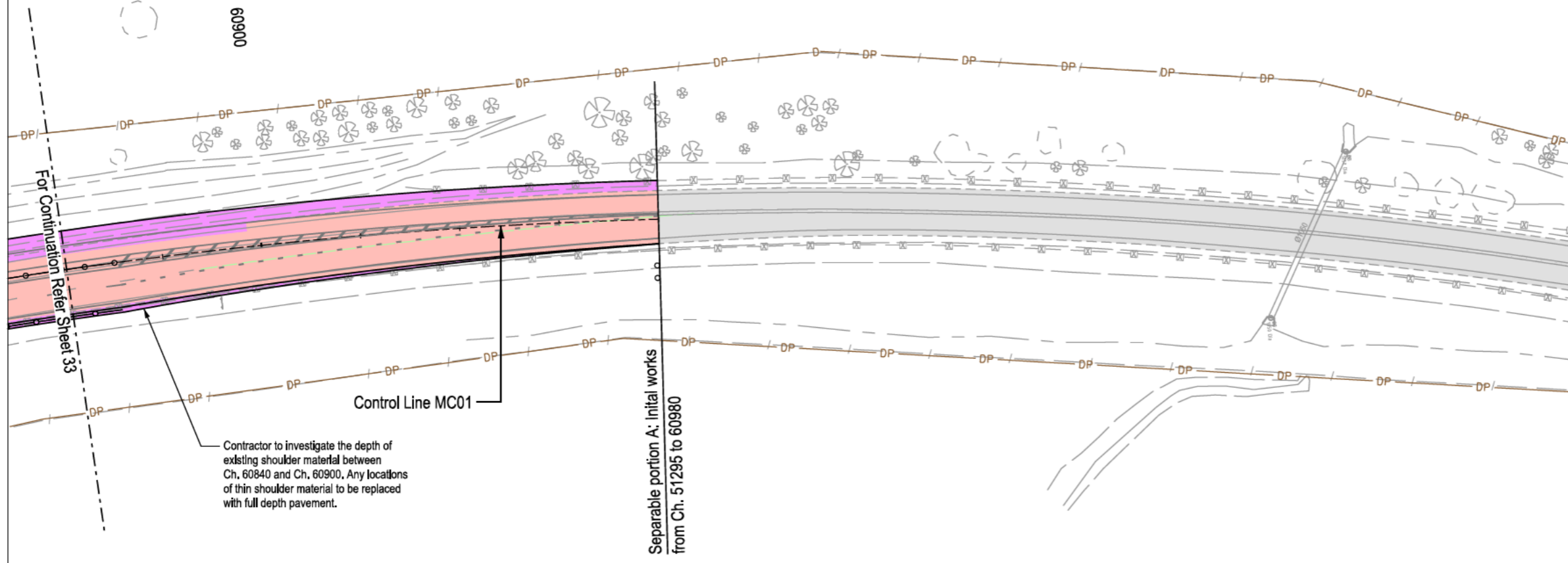
REVIEWED

Department of State Growth
MIDLAND HIGHWAY (A0087)
JERICO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
PAVEMENT EXTENTS PLAN
SHEET 33

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4633	PRINTED DATE 19/11/2021	SHEET No. 4633
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains



To Jericho

- Legend**
- DP — Property boundary
 - - - Existing Fence line
 - / — Proposed fence line
 - Pavement Type 1a - Road widening
 - Pavement Type 1b - Road widening
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SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

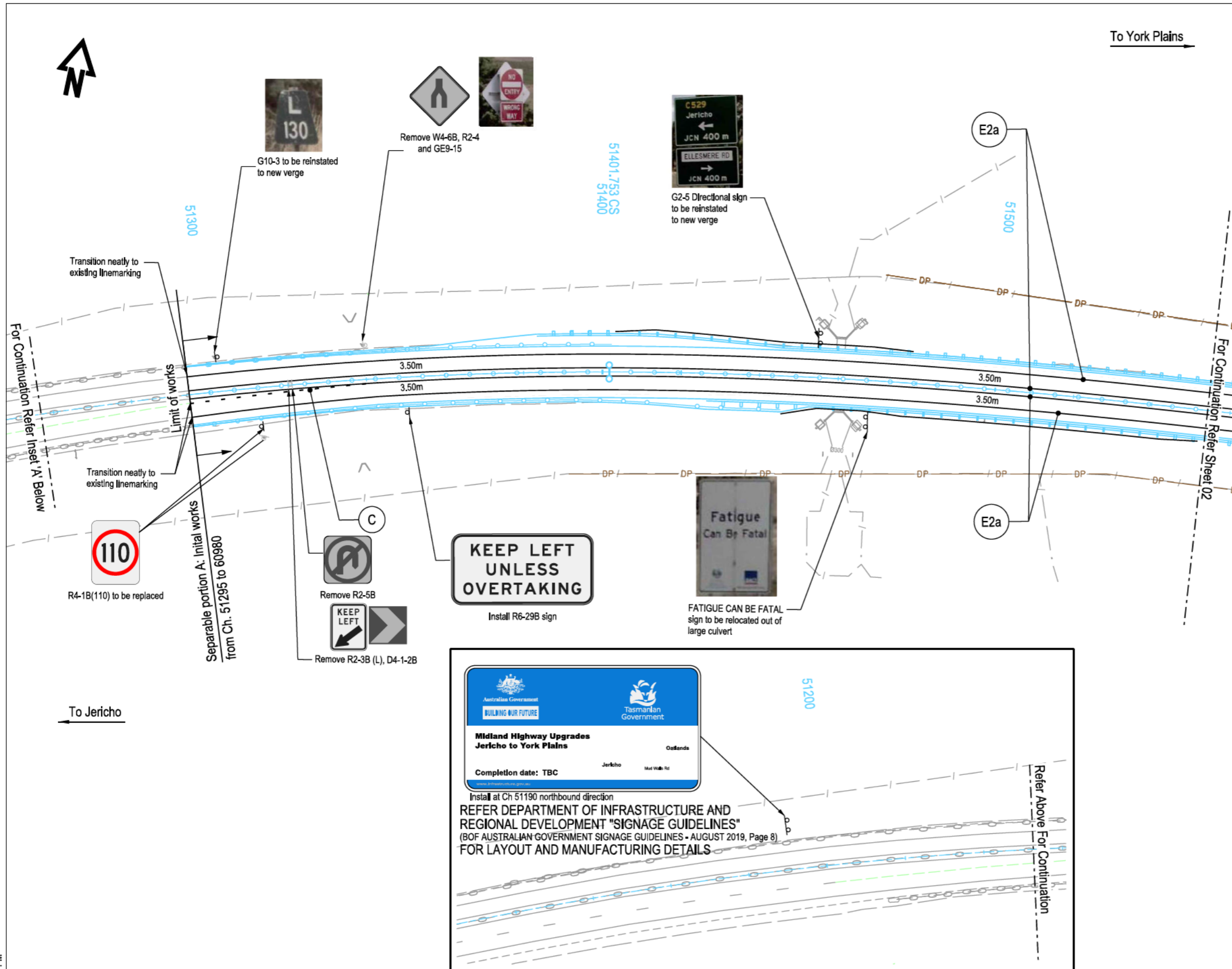
DESIGNED Out of Scope

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 PAVEMENT EXTENTS PLAN
 SHEET 34

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4634	PRINTED DATE 19/11/2021	SHEET No. 4634
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

- Lane (rural) (L)
- Edge rural (E2 & E2a)
- - - Edge continuity (rural) (EC2)
- - - Continuity (C)
- Barrier (both directions) (B2a)
- Continuous continuity (CC)
- - - Holding (HL)
- ↘ Arrow - merge (rural)
- ↙ Arrow - Turn (rural)
- Arrow - Straight Ahead
- ⊙ E2 ⊙ EC2 Linemarking identification code
- Existing sign (large)
- Existing sign (single)
- Proposed sign (large)
- Proposed sign (single)
- ⊞ Proposed sign (double)
- Functional Design
- Proposed Fence line
- DP Property boundary

- Notes**
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 3. Redundant pavement markings and RRPM's are to be removed to leave a clean, undamaged pavement with a surface texture, reflective characteristics and colour comparable to adjacent pavement surface.
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 - 5.4. Audio - tactile marking shall be 'Type B' for both asphalt & sprayed seals.
 - 5.5. Straight pavement arrows used for defining two-way traffic shall be 8m in length.
 6. Arrow spacing as per DSG standard drawing SD-84.015 and SD-84.018 unless noted otherwise.
 7. Provide RRPM's and guide posts in accordance with D.S.G. standard drawings and the Specification.
 8. All existing signs are to be removed or relocated as denoted on drawings unless noted otherwise.
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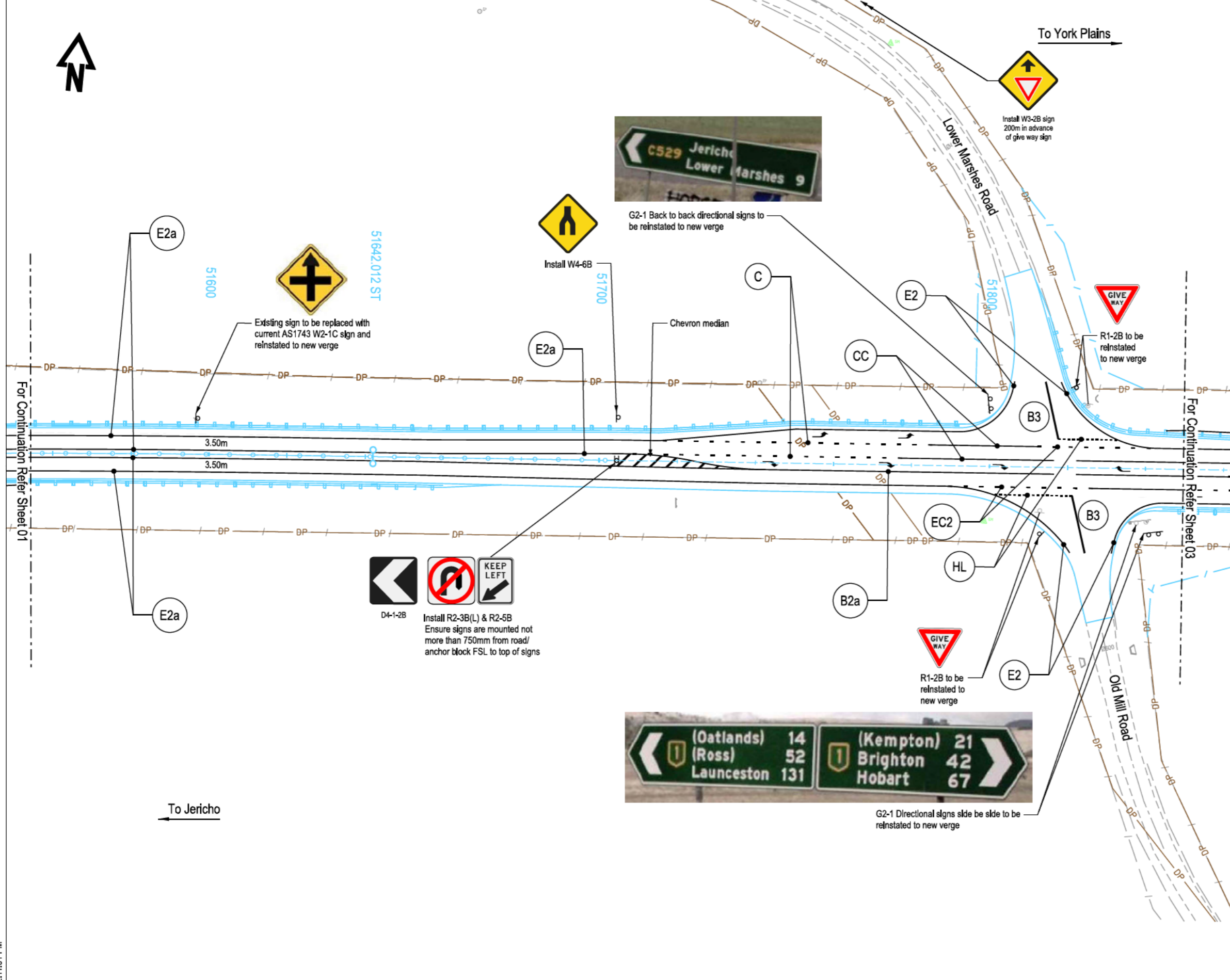
Warning
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INSET 'A'

Install at Ch 51190 northbound direction
 REFER DEPARTMENT OF INFRASTRUCTURE AND REGIONAL DEVELOPMENT "SIGNAGE GUIDELINES"
 (BOF AUSTRALIAN GOVERNMENT SIGNAGE GUIDELINES - AUGUST 2019, Page 8)
 FOR LAYOUT AND MANUFACTURING DETAILS

DESIGN

	<p>SCALES</p> <p>SCALE 1:1000 (A3)</p>			<p>Department of State Growth</p> <p>MIDLAND HIGHWAY (A0087) JERICO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE AND PAVEMENT MARKING PLAN SHEET 1</p>	<p>CONTRACT No. 3169</p>	<p>DRAWING NO. IA250204-OAT-4000-CR-DRG-4801</p>	<p>PRINTED DATE 19/11/2021</p>	<p>SHEET No. 4801</p>
					<p>REGISTRATION NUMBER A0087.059</p>			<p>REVISION 0</p>



Legend

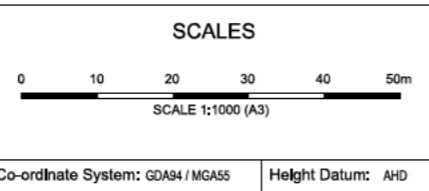
- Lane (rural) (L)
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- - - Edge continuity (rural) (EC2)
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DESIGN

0	DESIGN ISSUE	J.E.	19/11/21
No.	Amendment Description	Initials	Date
A3 original	This sheet may be prepared using colour and may be incomplete if copied		



Jacobs

Tasmanian Government

Out of Scope

DESIGNED

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 2

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4802	PRINTED DATE 19/11/2021	SHEET No. 4802
REGISTRATION NUMBER A0087.059			REVISION 0



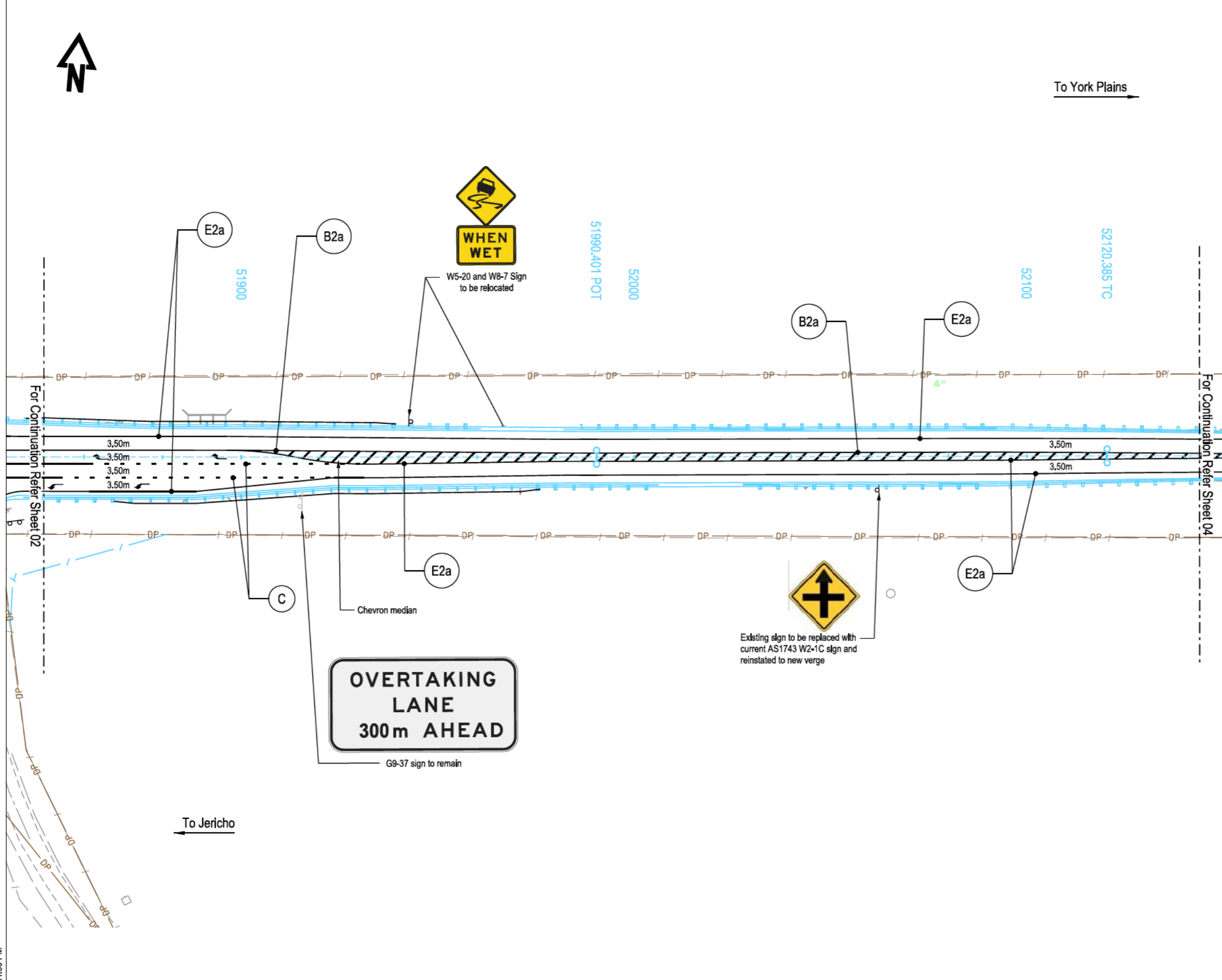
To York Plains →

← To Jericho

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
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DESIGN

19/11/2021 12:11:50 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED Out of Scope

REVIEWED

Department of State Growth

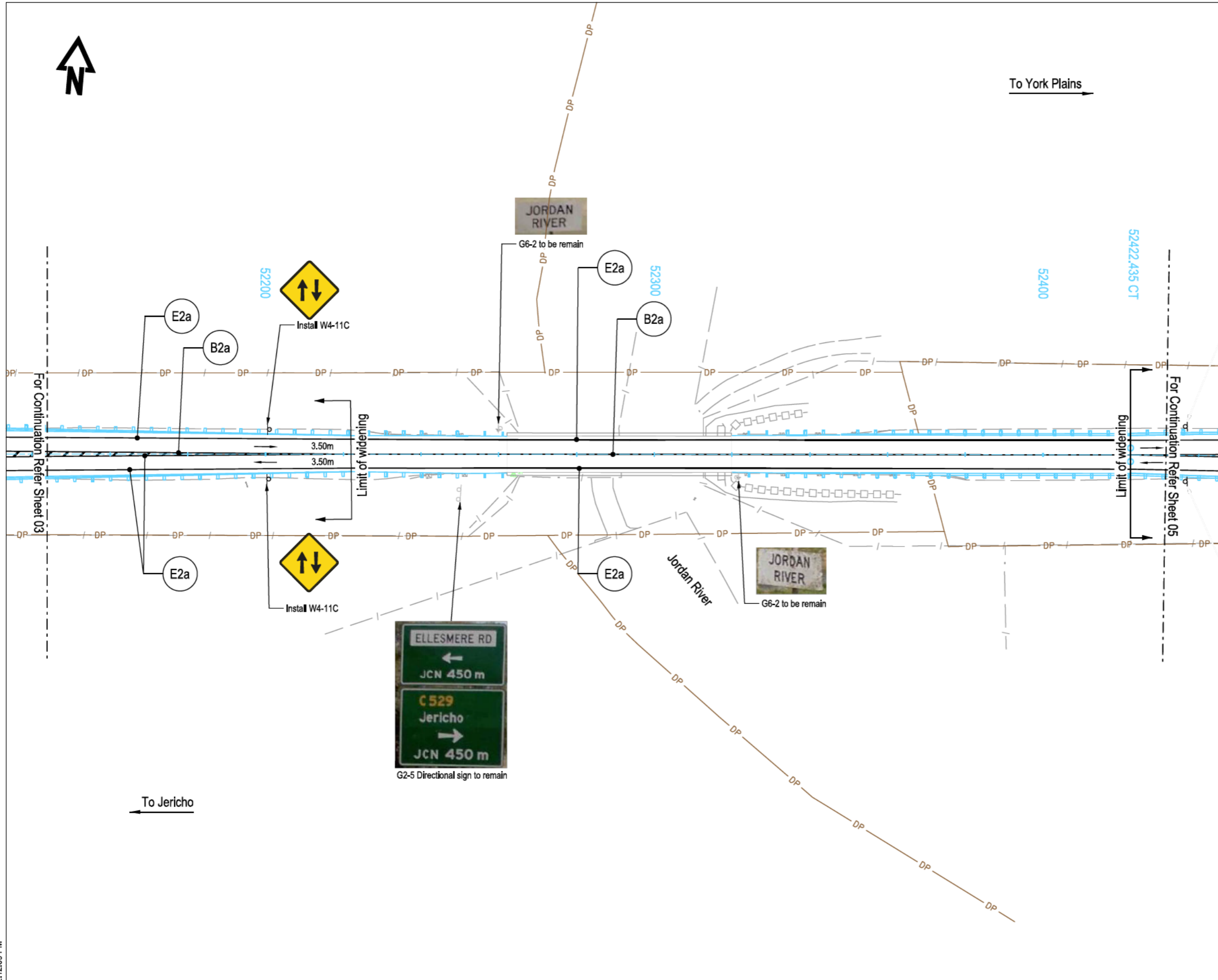
MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 3

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4803	PRINTED DATE 19/11/2021	SHEET No. 4803
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains →

← To Jericho



Legend

- Lane (rural) (L)
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DESIGN

19/11/2021 12:12:09 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

Out of Scope

DESIGNED
REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 4

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4804	PRINTED DATE 19/11/2021	SHEET No. <b style="font-size: 1.5em;">4804
REGISTRATION NUMBER <b style="font-size: 1.5em;">A0087.059			REVISION 0



To York Plains →

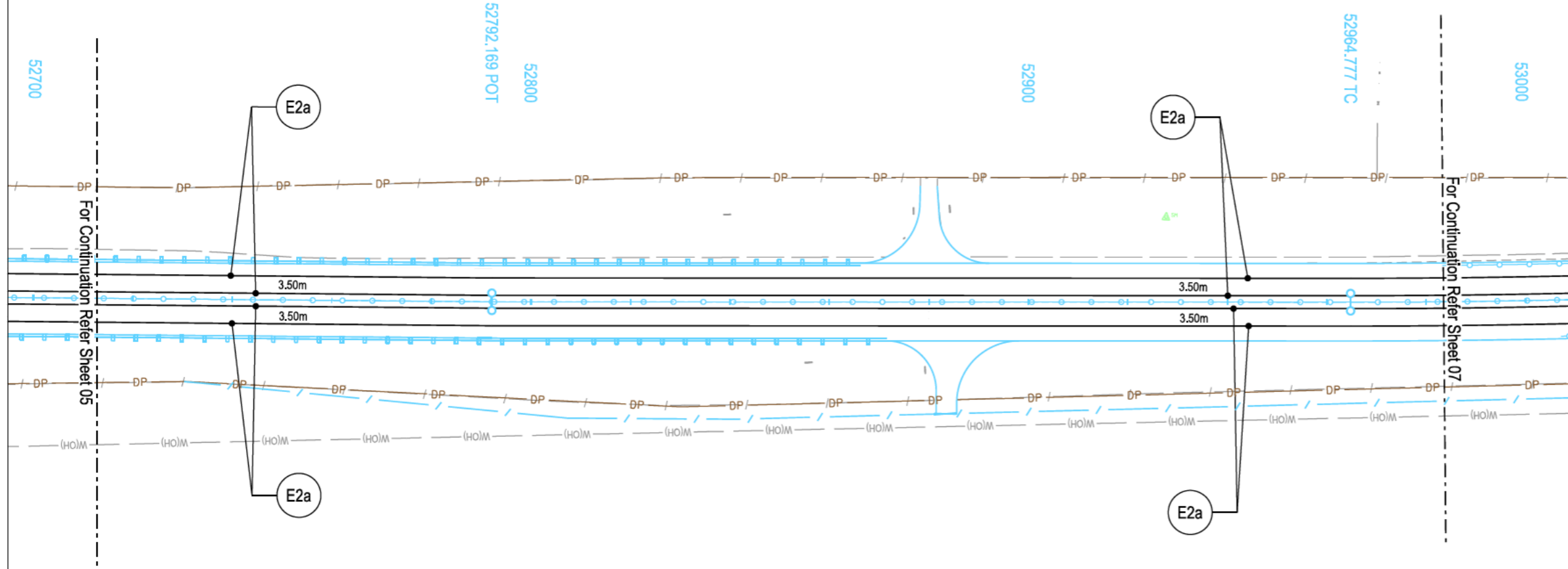
← To Jericho

- Legend**
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 - ⊙ E2 ⊙ EC2 LInemarking Identification code
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DESIGN



19/11/2021 12:12:48 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

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SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

Tasmanian Government

DESIGNED: [Redacted] **Out of Scope**

REVIEWED: [Redacted]

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 6

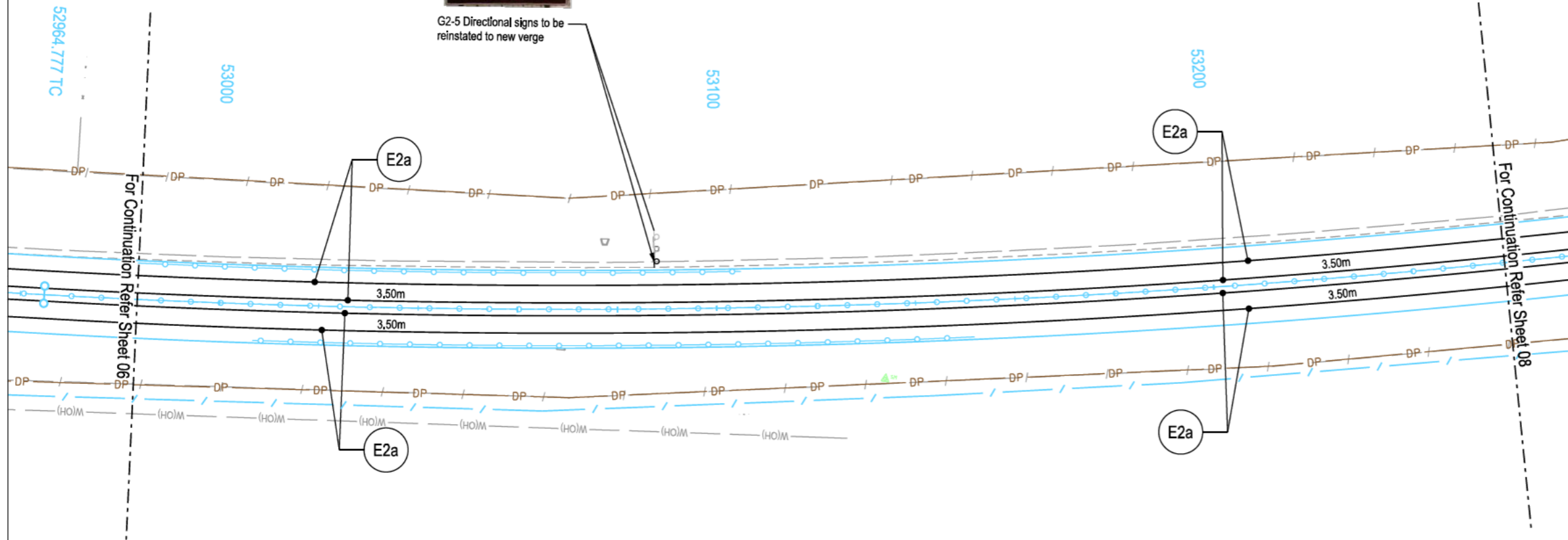
CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4806	PRINTED DATE 19/11/2021	SHEET No. 4806
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains →



G2-5 Directional signs to be reinstated to new verge



← To Jericho

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DESIGN

19/11/2021 12:13:06 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

A3 original This sheet may be prepared using colour and may be incomplete if copied

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**

REVIEWED

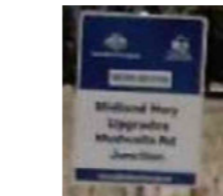
Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 7

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4807	PRINTED DATE 19/11/2021	SHEET No. 4807
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains



Midland HWY upgrades mudwalls rd junction sign to be removed



R6-10-2 (Modified) to be reinstated to new verge



GE4-1 Directional signs to be reinstated to new verge

Legend

—	Lane (rural) (L)
—	Edge rural (E2 & E2a)
- - - - -	Edge continuity (rural) (EC2)
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DESIGN

19/11/2021 12:13:25 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55
 Height Datum: AHD

Jacobs
 Tasmanian Government

DESIGNED **Out of Scope**
 REVIEWED

Department of State Growth
 MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 8

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4808	PRINTED DATE 19/11/2021	SHEET No. 4808
REGISTRATION NUMBER A0087.059			REVISION 0



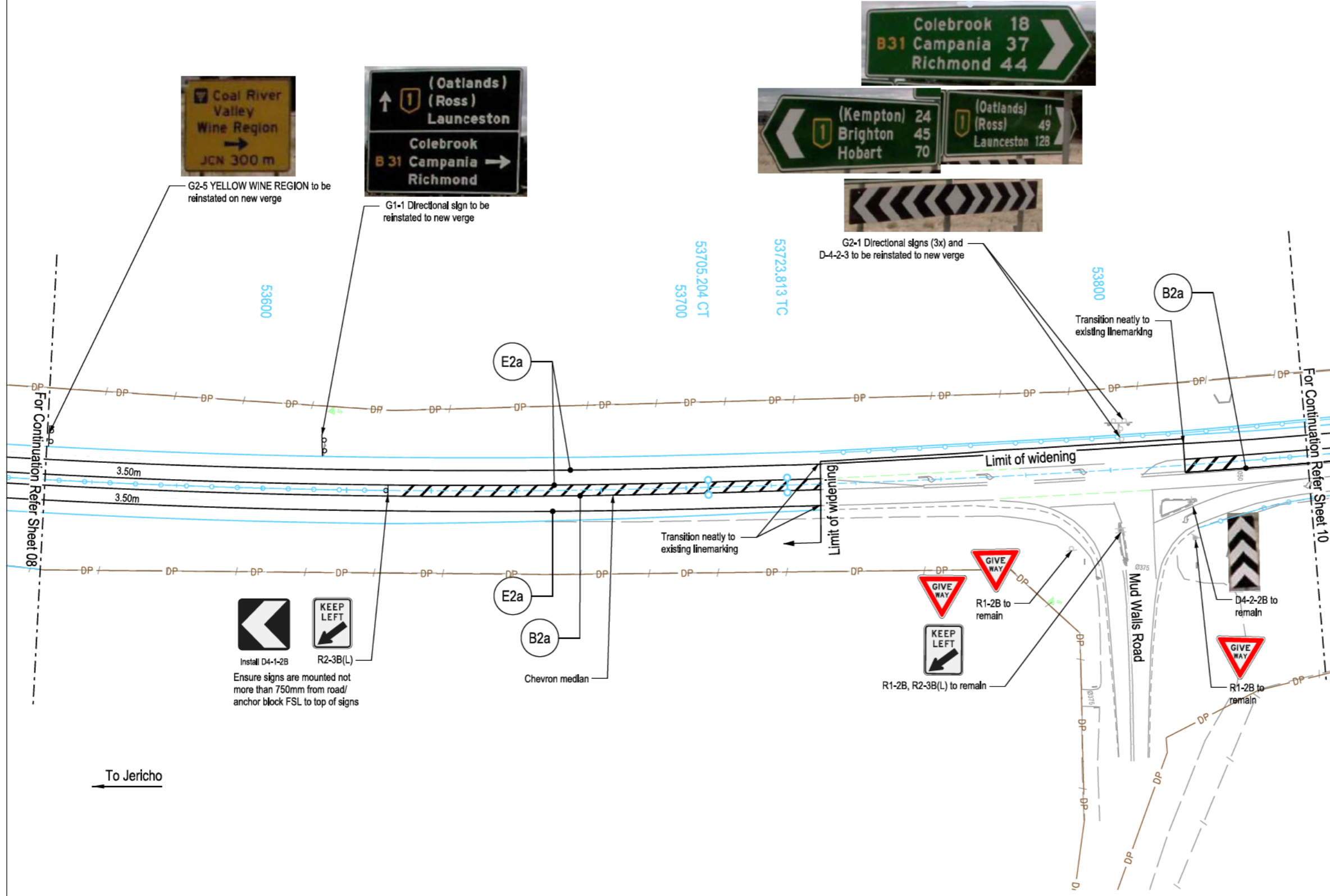
To York Plains

Legend

—	Lane (rural) (L)
—	Edge rural (E2 & E2a)
- - - - -	Edge continuity (rural) (EC2)
- - - - -	Continuity (C)
—	Barrier (both directions) (B2a)
—	Continuous continuity (CC)
- - - - -	Holding (HL)
↘	Arrow - merge (rural)
↙	Arrow - Turn (rural)
→	Arrow - Straight Ahead
⊙ E2 ⊙	Linemarking Identification code
⊙	Existing sign (large)
⊙	Existing sign (single)
⊙	Proposed sign (large)
⊙	Proposed sign (single)
⊙	Proposed sign (double)
—	Functional Design
—	Proposed Fence line
— DP —	Property boundary

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To Jericho

19/11/2021 12:13:46 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55
Height Datum: AHD

Jacobs
Tasmanian Government

DESIGNED
REVIEWED

Out of Scope

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 9

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4809	PRINTED DATE 19/11/2021	SHEET No. 4809
REGISTRATION NUMBER A0087.059			REVISION 0

DESIGN



To York Plains →

← To Jericho

Legend

—	Lane (rural) (L)
—	Edge rural (E2 & E2a)
- - - -	Edge continuity (rural) (EC2)
- - - -	Continuity (C)
—	Barrier (both directions) (B2a)
—	Continuous continuity (CC)
- - - -	Holding (HL)
↘	Arrow - merge (rural)
↙	Arrow - Turn (rural)
→	Arrow - Straight Ahead
⊙ E2 ⊙	Linemarking identification code
○	Existing sign (large)
○	Existing sign (single)
○	Proposed sign (large)
○	Proposed sign (single)
⊞	Proposed sign (double)
—	Functional Design
—	Proposed Fence line
DP	Property boundary

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DESIGN

Install R2-3B(L) D4-1-2B
 Ensure signs are mounted not more than 750mm from road/ anchor block FSL to top of signs



G4-1 Directional sign to be reinstated to new verge



R4-1B(110) to be reinstated to new verge



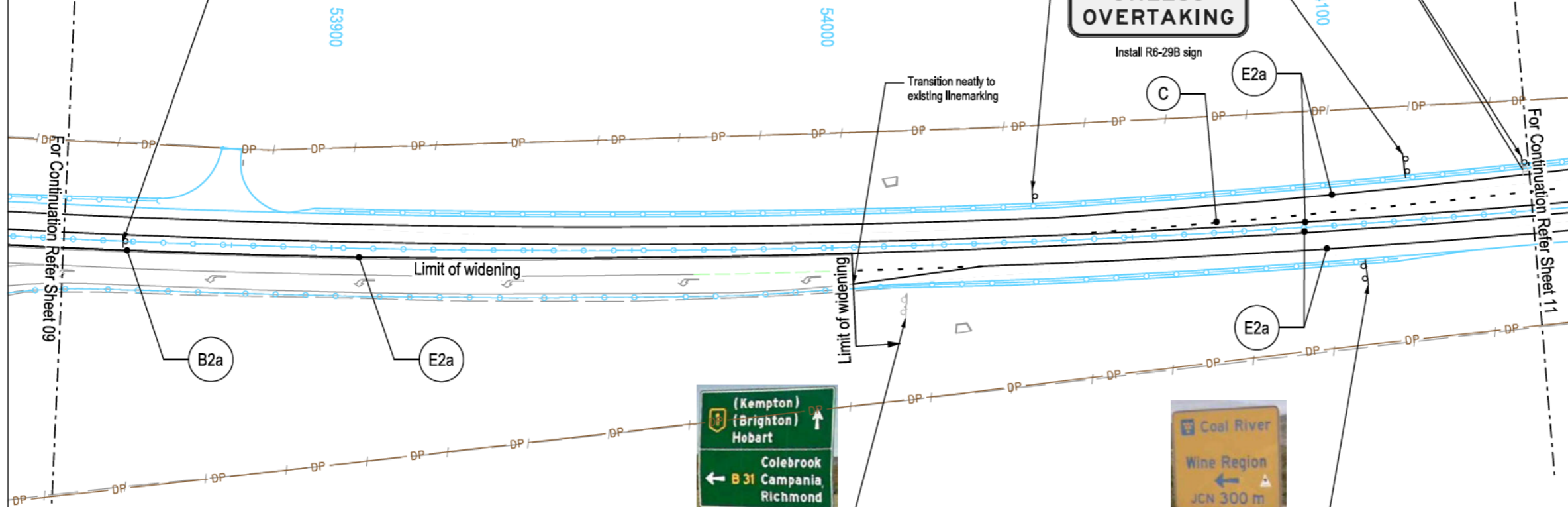
Install R6-29B sign



G1-1 Directional sign to remain



YELLOW WINE REGION to be reinstated to new verge



19/11/2021 12:14:05 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55
 Height Datum: AHD

Jacobs

Tasmanian Government

DESIGNED: [Redacted] **Out of Scope**

REVIEWED: [Redacted]

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 10

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4810	PRINTED DATE 19/11/2021	SHEET No. 4810
REGISTRATION NUMBER A0087.059			REVISION 0

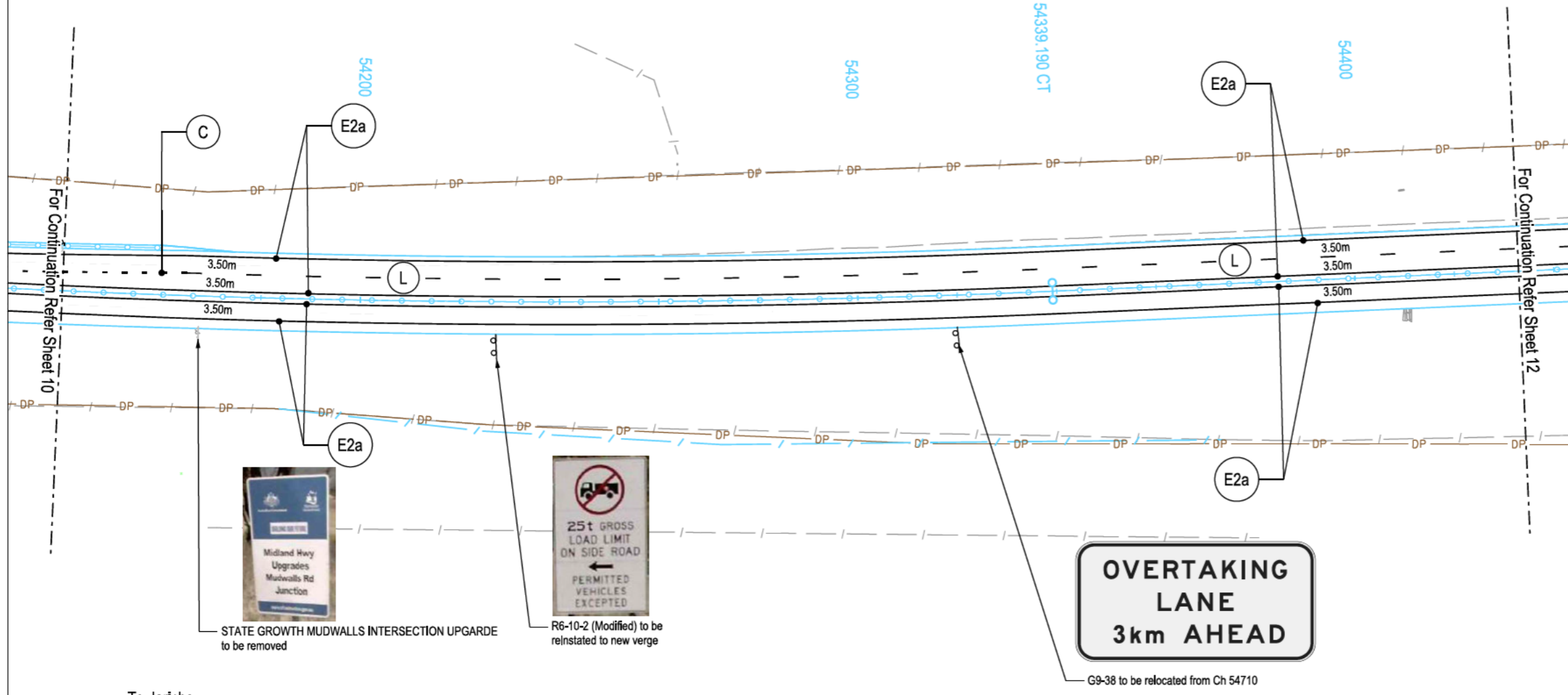


To York Plains →

Legend	
—	Lane (rural) (L)
—	Edge rural (E2 & E2a)
- - - - -	Edge continuity (rural) (EC2)
- - - - -	Continuity (C)
—	Barrier (both directions) (B2a)
—	Continuous continuity (CC)
-----	Holding (HL)
↘	Arrow - merge (rural)
↙	Arrow - Turn (rural)
→	Arrow - Straight Ahead
⊙ E2 ⊙	Linemarking identification code
⊙	Existing sign (large)
⊙	Existing sign (single)
⊙	Proposed sign (large)
⊙	Proposed sign (single)
⊙	Proposed sign (double)
—	Functional Design
—	Proposed Fence line
DP	Property boundary

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STATE GROWTH MUDWALLS INTERSECTION UPGARDE to be removed

R6-10-2 (Modified) to be reinstated to new verge

G9-38 to be relocated from Ch 54710

← To Jericho

DESIGN

19/11/2021 12:14:25 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55
Height Datum: AHD

Jacobs

DESIGNED: [Redacted] **Out of Scope**

REVIEWED: [Redacted]

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 11

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4811	PRINTED DATE 19/11/2021	SHEET No. 4811
REGISTRATION NUMBER A0087.059			REVISION 0

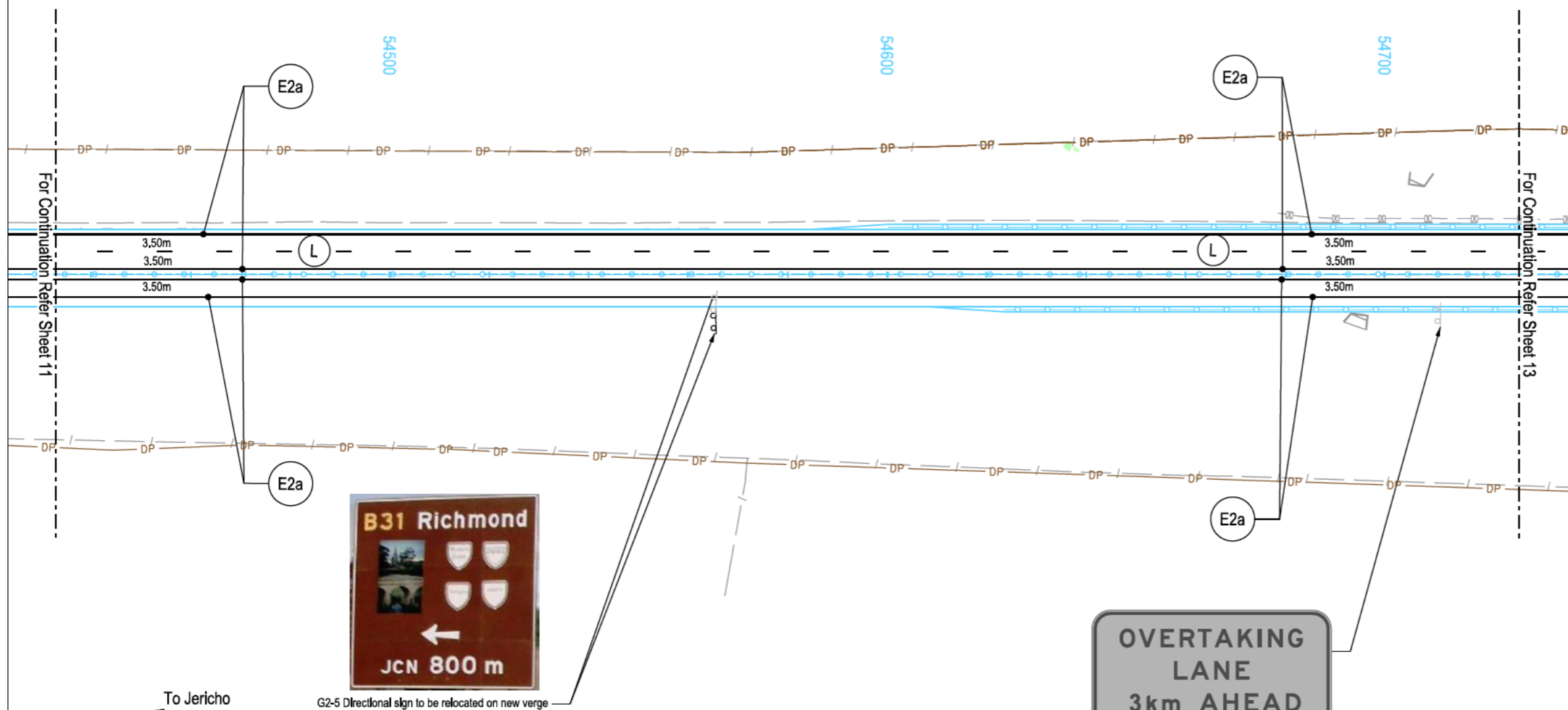


To York Plains →

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
 - ↙ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊠ Proposed sign (double)
 - Functional Design
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G2-5 Directional sign to be relocated on new verge



G9-38 Sign to be relocated at Ch 54320

DESIGN

19/11/2021 12:14:45 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 12

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4812	PRINTED DATE 19/11/2021	SHEET No. 4812
REGISTRATION NUMBER A0087.059			REVISION 0



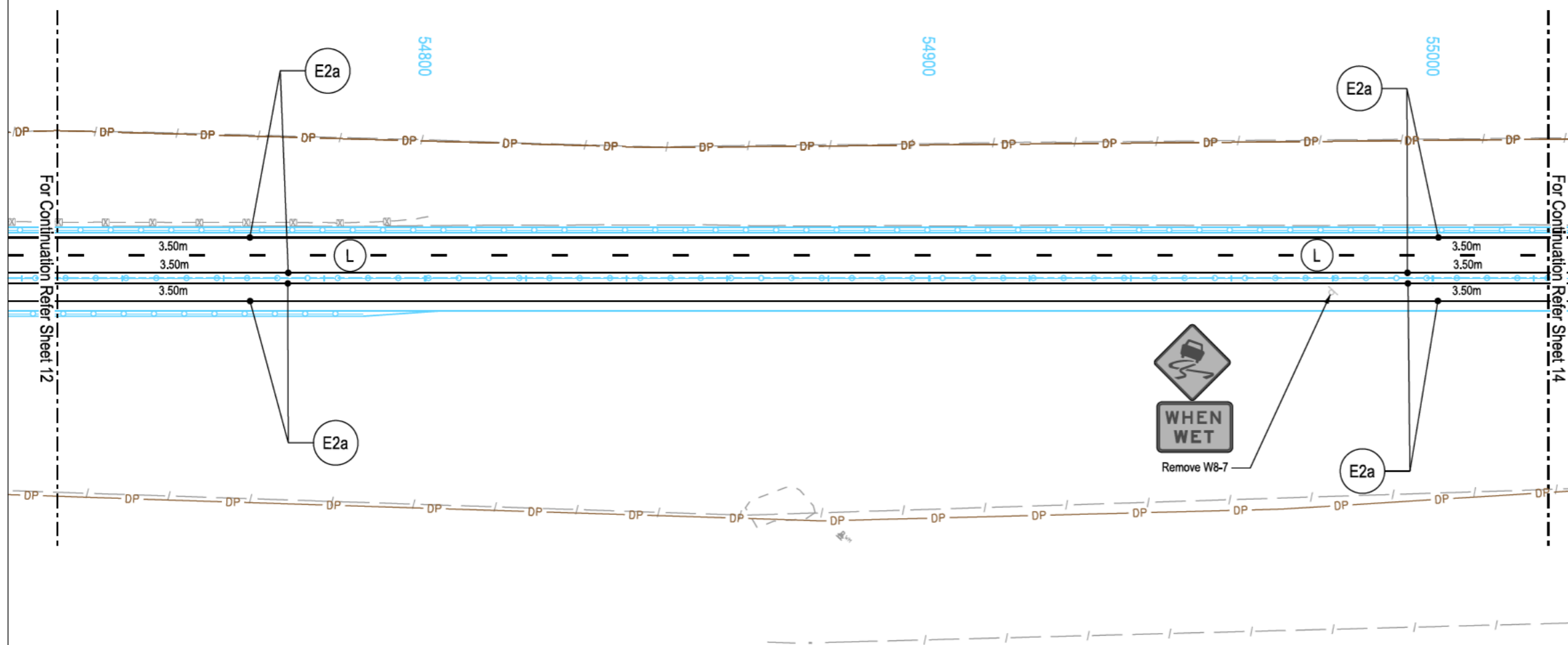
To York Plains →

← To Jericho

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
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 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
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 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - Existing sign (large)
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DESIGN

19/11/2021 12:15:03 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

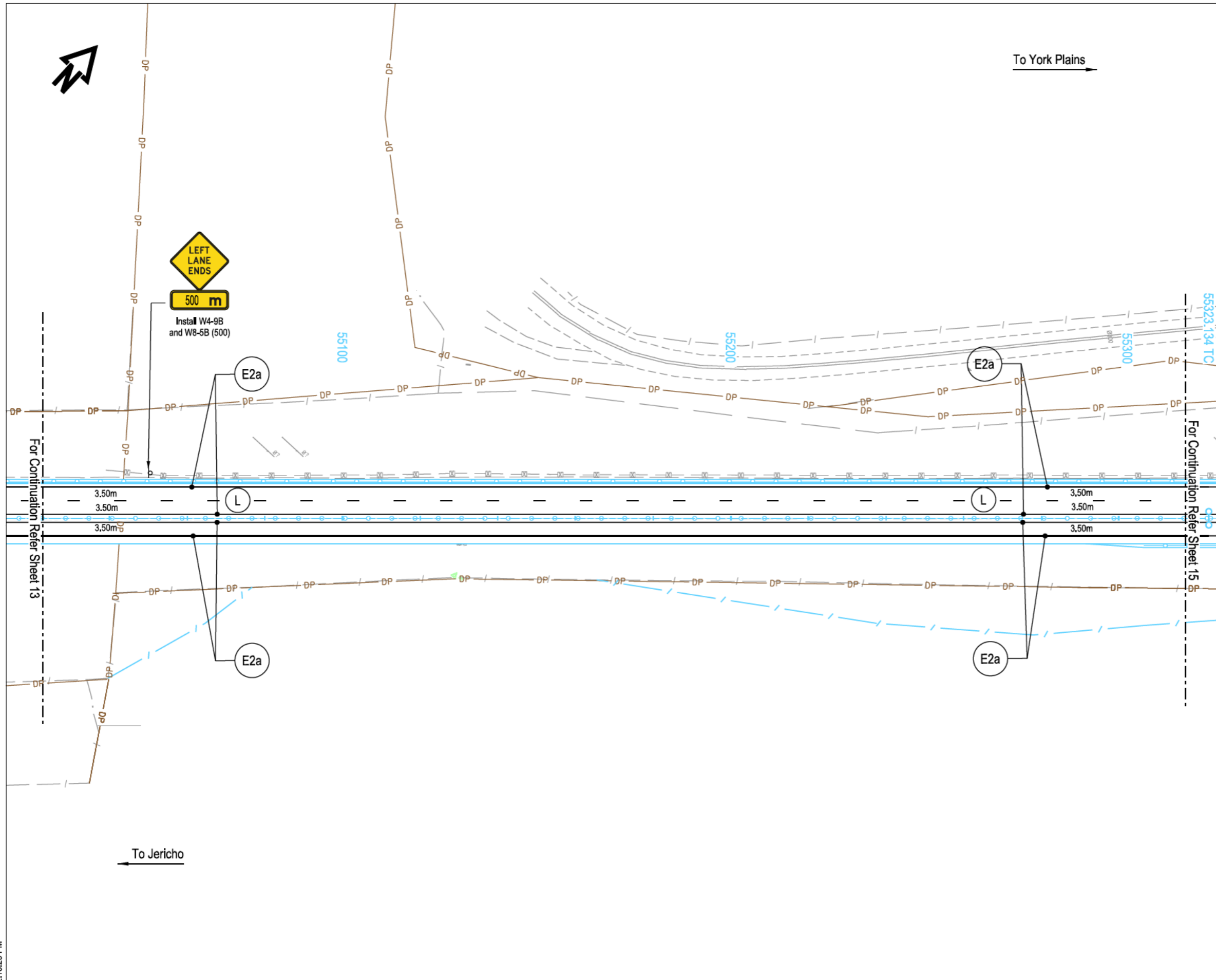
DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 13

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4813	PRINTED DATE 19/11/2021	SHEET No. 4813
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

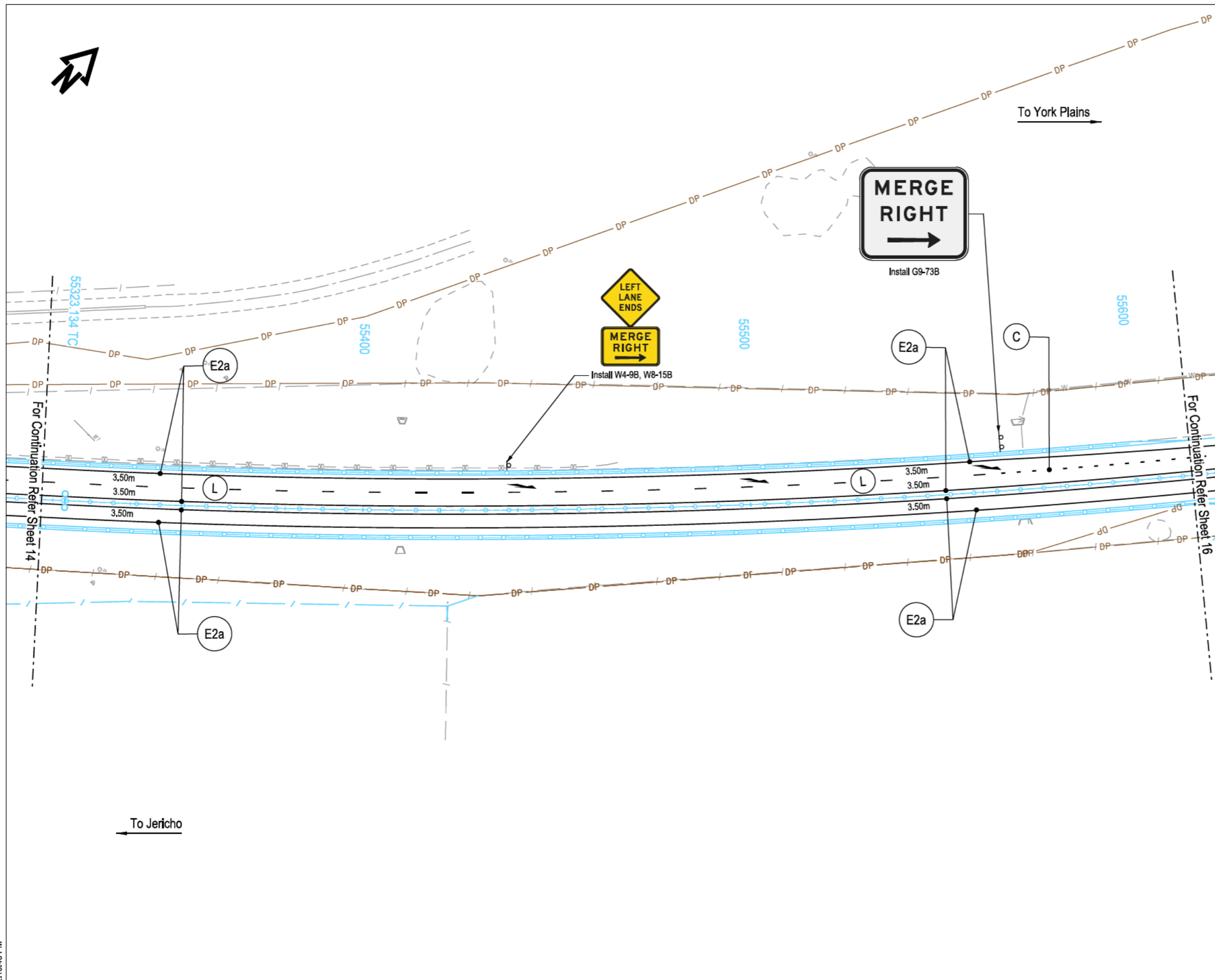
- Lane (rural) (L)
- Edge rural (E2 & E2a)
- - - - Edge continuity (rural) (EC2)
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- ⊙ E2 ⊙ EC2 LInemarking identification code
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DESIGN

	SCALES SCALE 1:1000 (A3)	 	Department of State Growth MIDLAND HIGHWAY (A0087) JERICHO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE AND PAVEMENT MARKING PLAN SHEET 14	CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4814	PRINTED DATE 19/11/2021	SHEET No. 4814
				REGISTRATION NUMBER A0087.059		REVISION 0	
0 DESIGN ISSUE No. Amendment Description A3 original This sheet may be prepared using colour and may be incomplete if copied	J.E. 19/11/21 Initials Date	Co-ordinate System: GDA94 / MGA55 Height Datum: AHD					



Legend	
	Lane (rural) (L)
	Edge rural (E2 & E2a)
	Edge continuity (rural) (EC2)
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	Arrow - merge (rural)
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DESIGN

19/11/2021 12:15:43 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

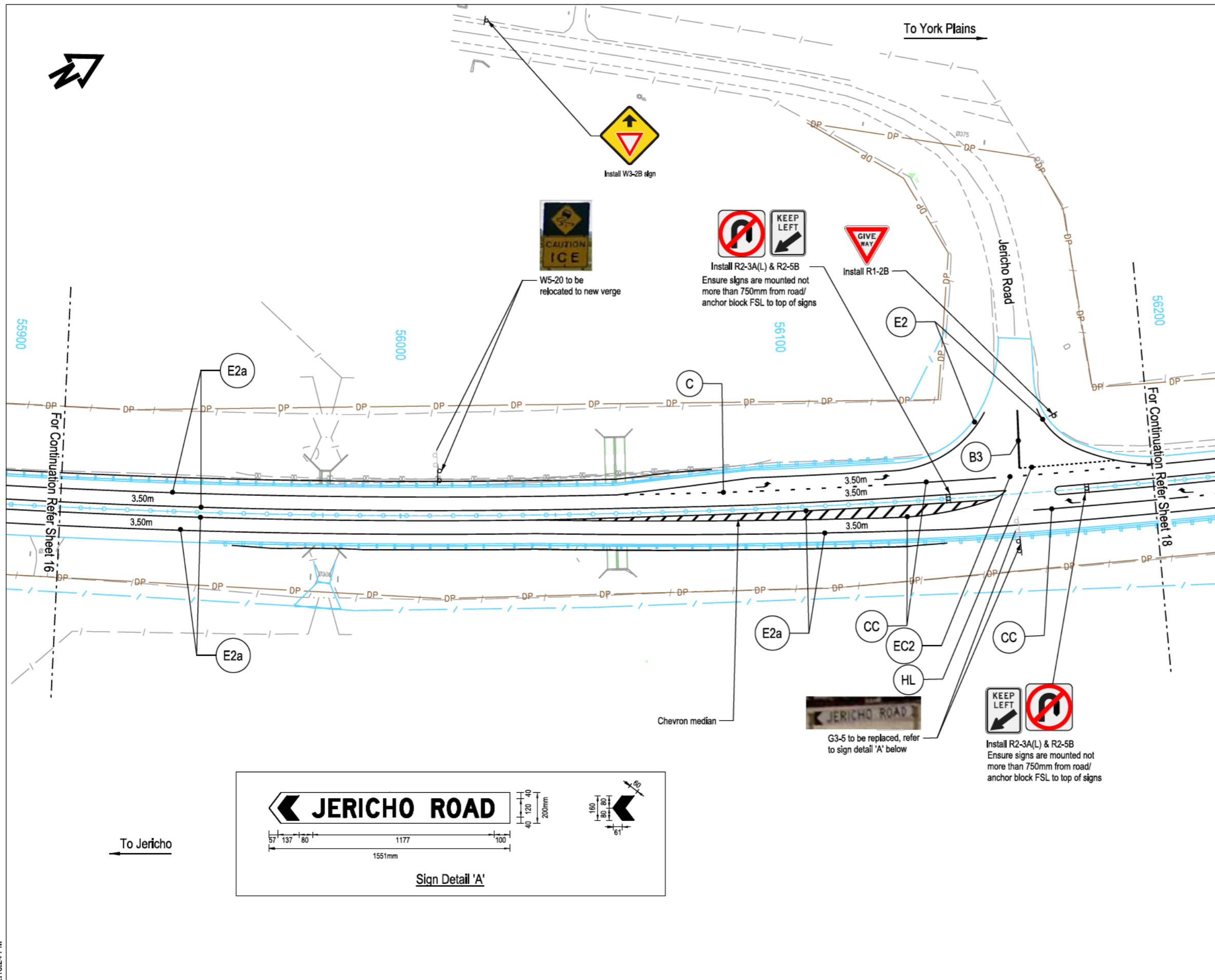
DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 15

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4815	PRINTED DATE 19/11/2021	SHEET No. 4815
REGISTRATION NUMBER A0087.059			REVISION 0

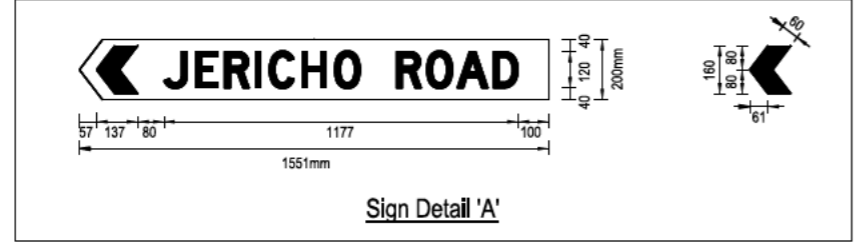


Legend

- Lane (rural) (L)
- Edge rural (E2 & E2a)
- - - Edge continuity (rural) (EC2)
- - - Continuity (C)
- Barrier (both directions) (B2a)
- Continuous continuity (CC)
- - - Holding (HL)
- ↘ Arrow - merge (rural)
- ↙ Arrow - Turn (rural)
- Arrow - Straight Ahead
- ⊙ E2 ⊙ EC2 LInemarking Identification code
- Existing sign (large)
- Existing sign (single)
- Proposed sign (large)
- Proposed sign (single)
- ⊞ Proposed sign (double)
- Functional Design
- - - Proposed Fence line
- DP Property boundary

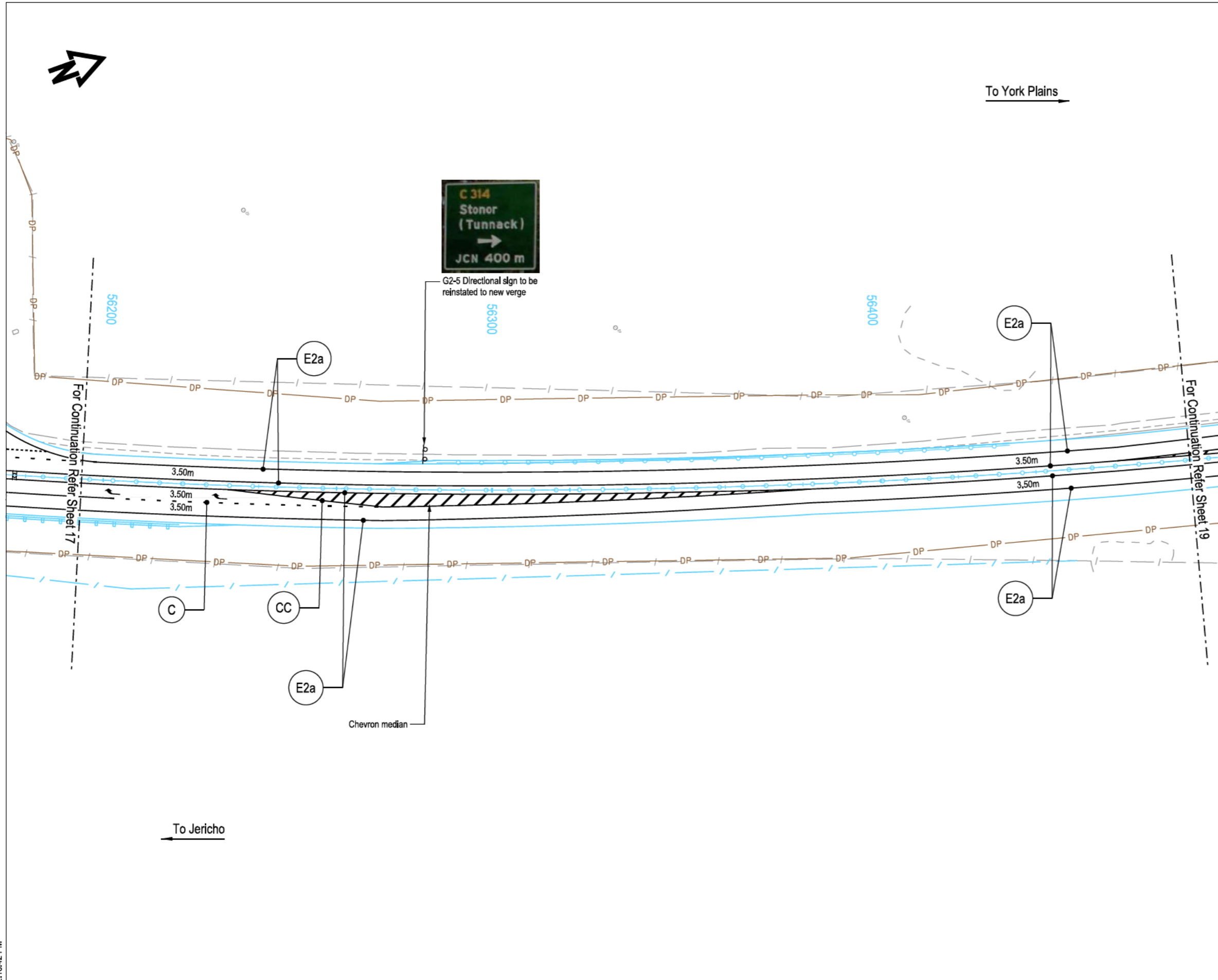
- Notes**
- The contractor shall locate all existing and relocated services prior to any construction related to traffic sign supports and footings.
 - Align new pavement marking with existing pavement markings at limits of work / widening.
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 - Arrow spacing as per DSG standard drawing SD-84.015 and SD-84.018 unless noted otherwise.
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DESIGN

	SCALES SCALE 1:1000 (A3)	 Out of Scope	Department of State Growth MIDLAND HIGHWAY (A0087) JERICHO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE AND PAVEMENT MARKING PLAN SHEET 17	CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4817	PRINTED DATE 19/11/2021	SHEET No. 4817 REVISION 0
No. 0 DESIGN ISSUE Amendment Description: This sheet may be prepared using colour and may be incomplete if copied	J.E. Initials	19/11/21 Date	Co-ordinate System: GDA94 / MGA55 Height Datum: AHD	REGISTRATION NUMBER A0087.059			



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
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 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - ⊙ Existing sign (large)
 - ⊙ Existing sign (single)
 - ⊙ Proposed sign (large)
 - ⊙ Proposed sign (single)
 - ⊙ Proposed sign (double)
 - Functional Design
 - - - - Proposed Fence line
 - - - - Property boundary

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DESIGN

19/11/2021 12:18:42 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 18

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4818	PRINTED DATE 19/11/2021	SHEET No. 4818
REGISTRATION NUMBER A0087.059			REVISION 0

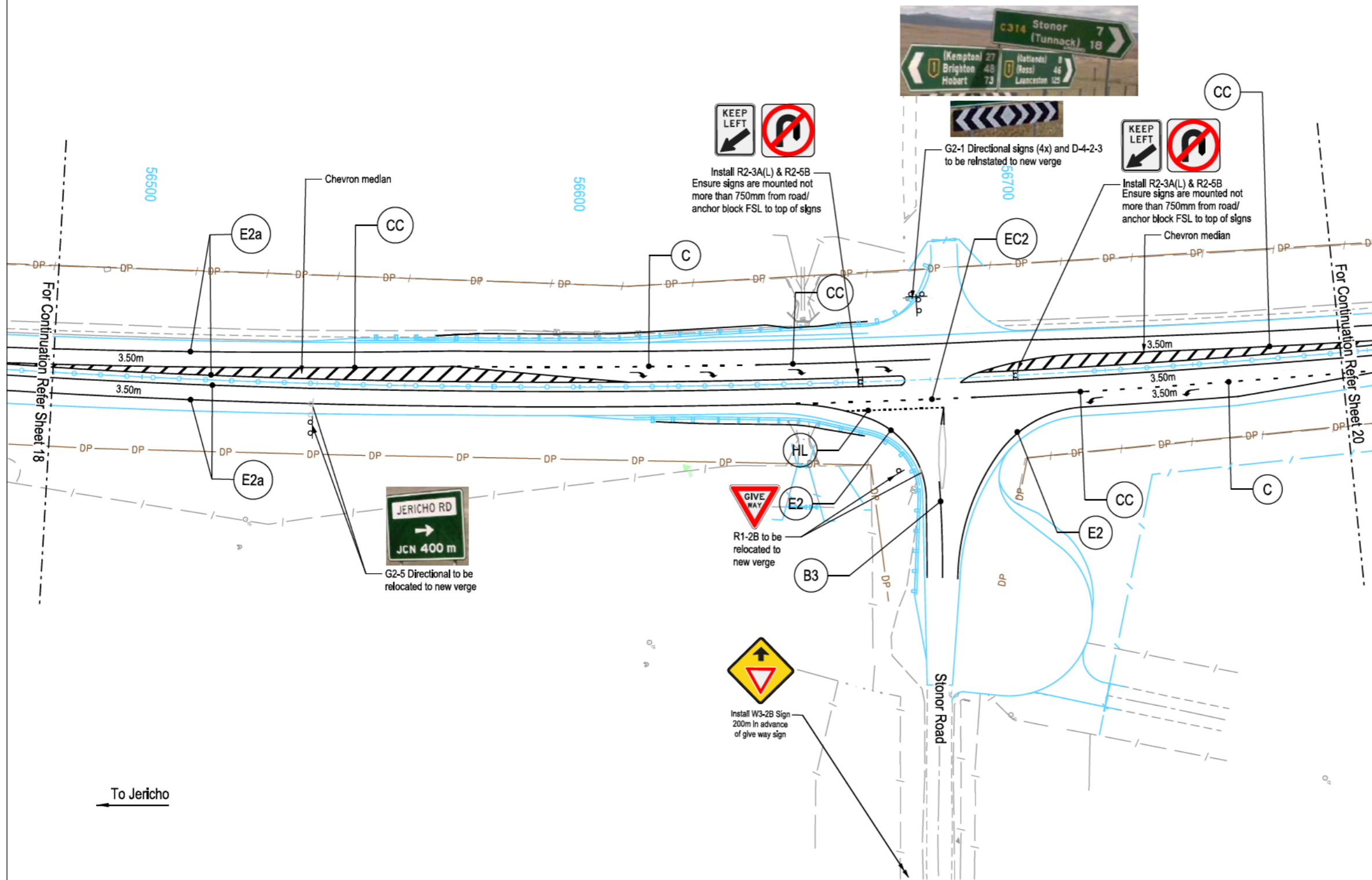


To York Plains

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - ==== Barrier (both directions) (B2a)
 - ==== Continuous continuity (CC)
 - Holding (HL)
 - ↘ Arrow - merge (rural)
 - ↙ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
 - Functional Design
 - Proposed Fence line
 - Property boundary

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To Jericho

DESIGN

19/11/2021 12:17:00 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

0 10 20 30 40 50m

SCALE 1:1000 (A3)

Jacobs

Tasmanian Government

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 19

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4819	PRINTED DATE 19/11/2021	SHEET No. 4819
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains

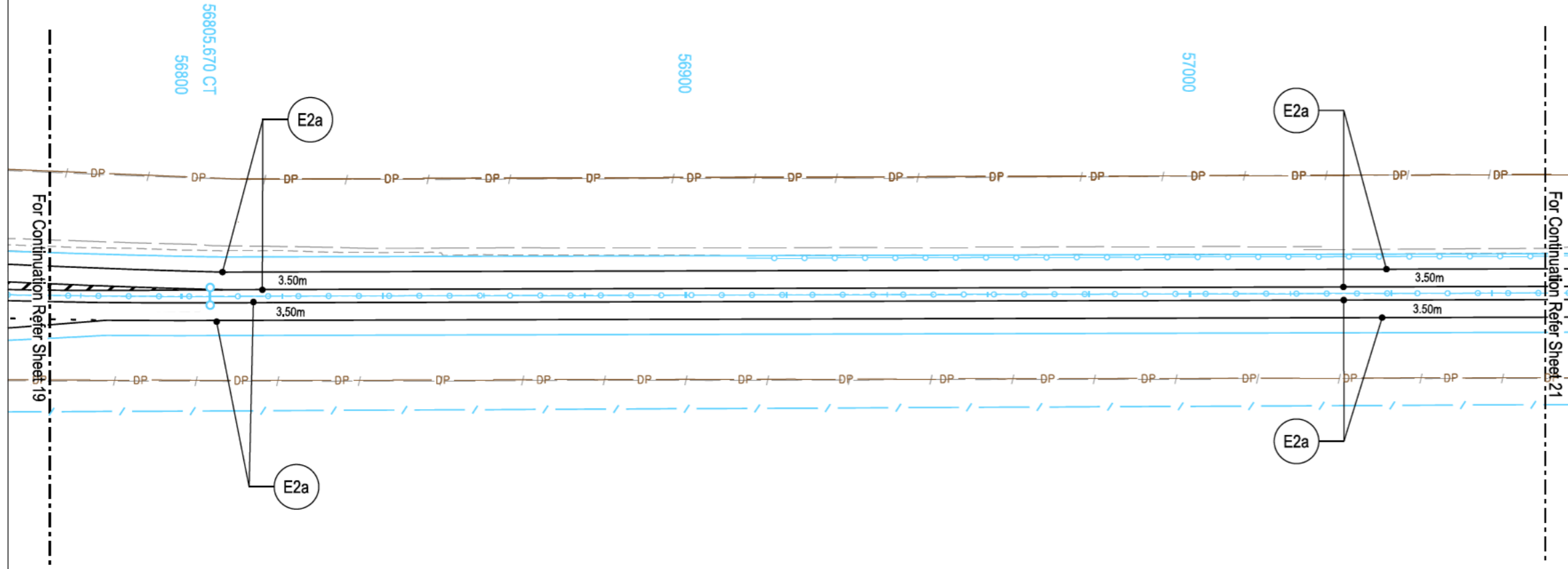
To Jericho

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
 - ↙ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
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DESIGN



19/11/2021 12:17:20 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55
 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**
 REVIEWED

Department of State Growth
 MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 20

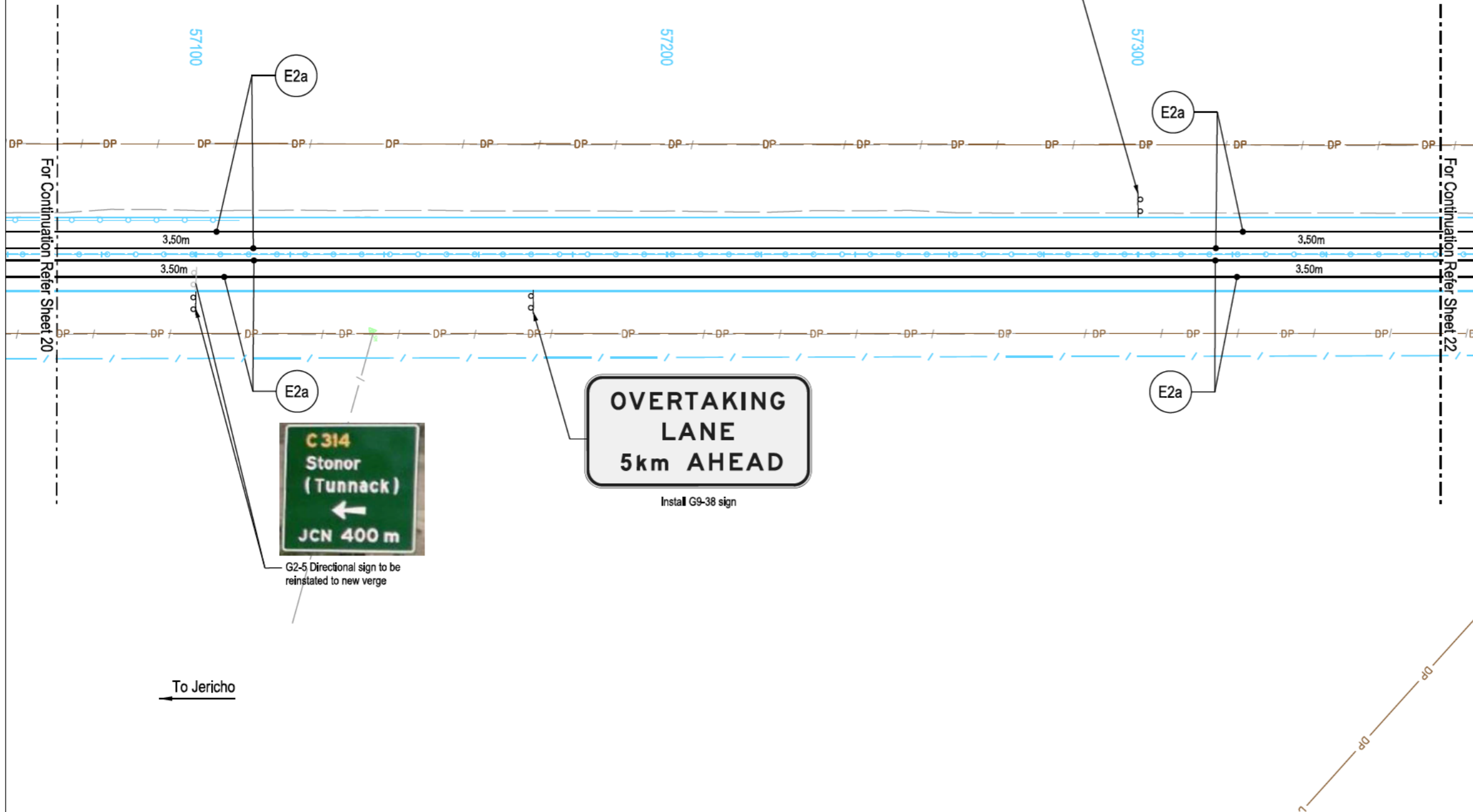
CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4820	PRINTED DATE 19/11/2021	SHEET No. 4820
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains

OVERTAKING LANE 2km AHEAD

G9-38 to be relocated from Ch 57450



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↙ Arrow - merge (rural)
 - ↘ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - ⊙ Existing sign (large)
 - ⊙ Existing sign (single)
 - ⊙ Proposed sign (large)
 - ⊙ Proposed sign (single)
 - ⊙ Proposed sign (double)
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DESIGN

12:17:37 PM

19/11/2021

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

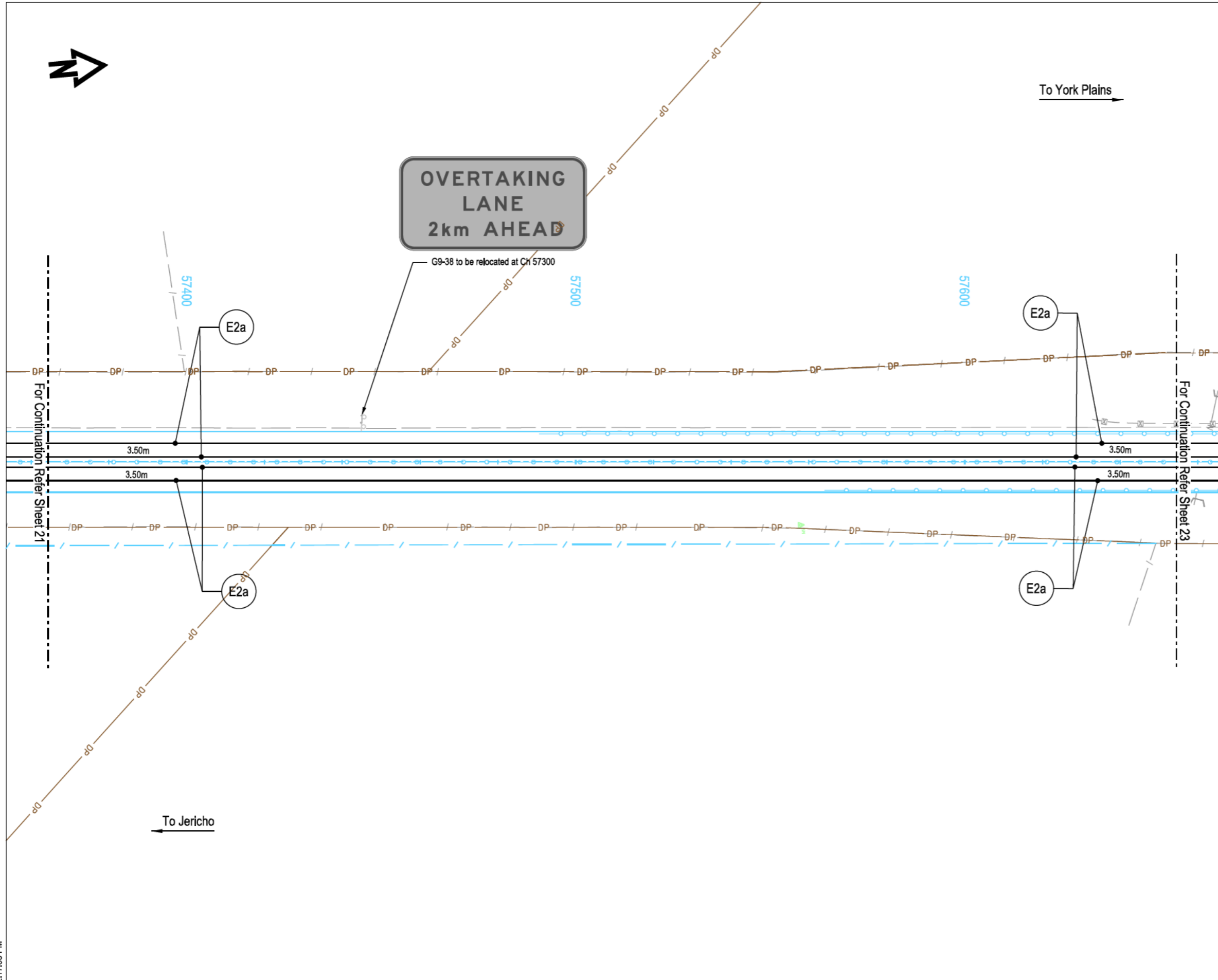
DESIGNED Out of Scope

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 21

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4821	PRINTED DATE 19/11/2021	SHEET No. 4821
REGISTRATION NUMBER A0087.059			REVISION 0



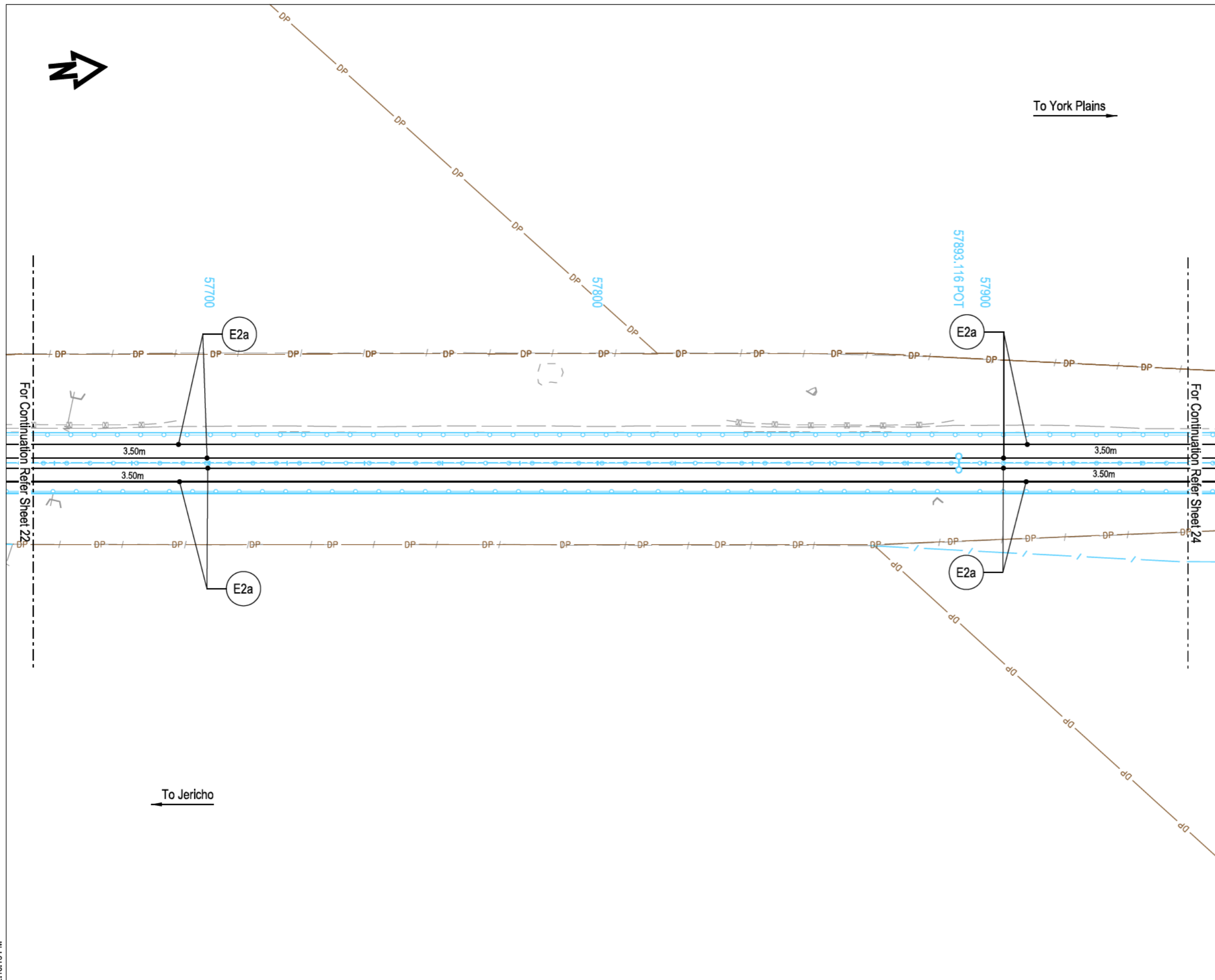
Legend	
	Lane (rural) (L)
	Edge rural (E2 & E2a)
	Edge continuity (rural) (EC2)
	Continuity (C)
	Barrier (both directions) (B2a)
	Continuous continuity (CC)
	Holding (HL)
	Arrow - merge (rural)
	Arrow - Turn (rural)
	Arrow - Straight Ahead
	Linemarking Identification code
	Existing sign (large)
	Existing sign (single)
	Proposed sign (large)
	Proposed sign (single)
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	Functional Design
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DESIGN

19/11/2021 12:17:58 PM	<p style="text-align: center;">SCALES</p> <p style="text-align: center;">SCALE 1:1000 (A3)</p>			 	<p style="text-align: center;">Department of State Growth</p> <p style="text-align: center;">MIDLAND HIGHWAY (A0087) JERICO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE AND PAVEMENT MARKING PLAN SHEET 22</p>	CONTRACT No.	DRAWING NO.	PRINTED DATE	SHEET No.
	0	DESIGN ISSUE	J.E.			19/11/21	3169	IA250204-OAT-4000-CR-DRG-4822	19/11/2021
No. Amendment Description Initials Date		Co-ordinate System: GDA94 / MGA55 Height Datum: AHD		DESIGNED REVIEWED	REGISTRATION NUMBER A0087.059		REVISION 0		



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
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 - ⊙ Existing sign (large)
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DESIGN

19/11/2021 12:18:18 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

Out of Scope

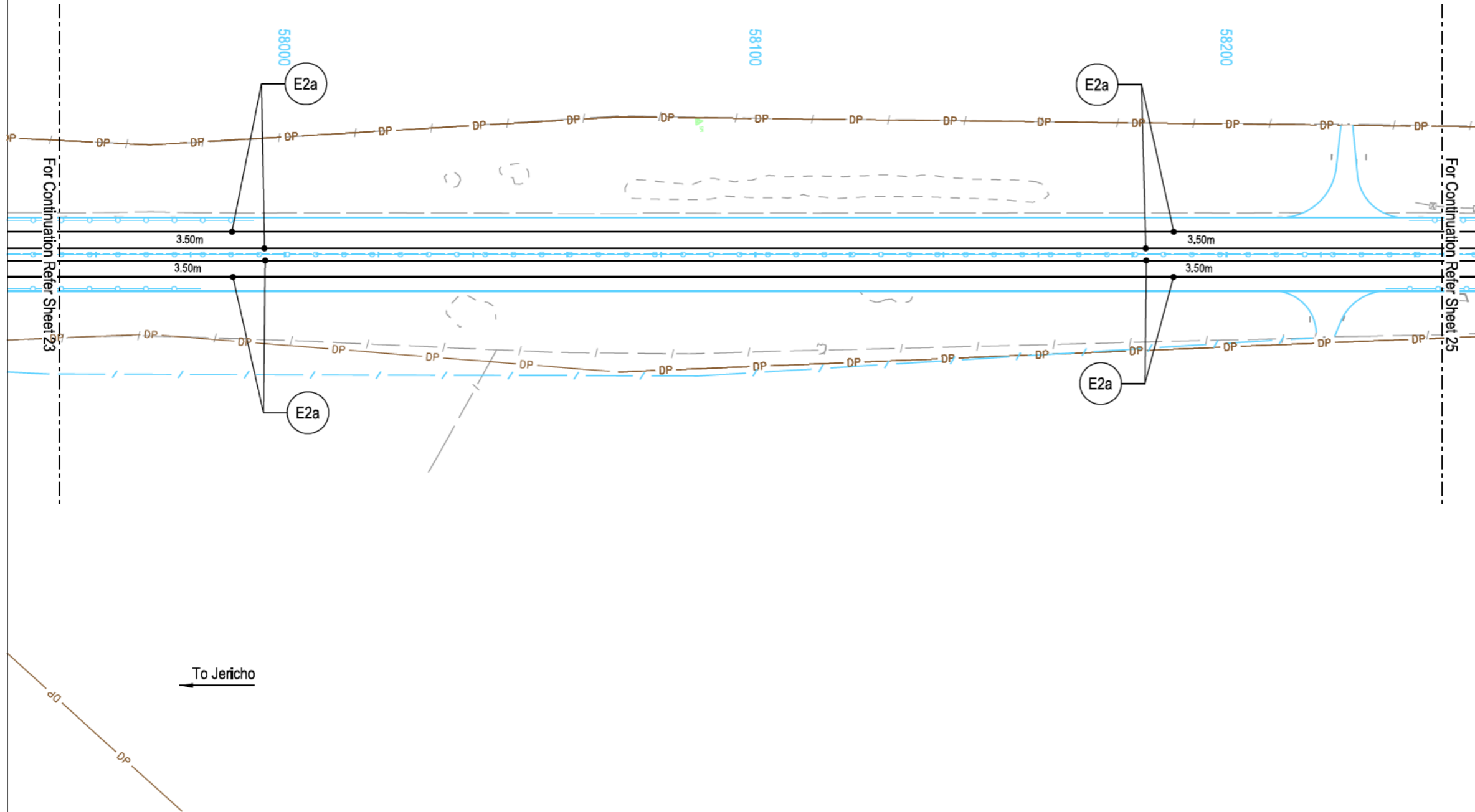
Department of State Growth
 MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 23

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4823	PRINTED DATE 19/11/2021	SHEET No. 4823
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains

To Jericho



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
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 12. Refer to D.S.G. Specification Section 709 for guide post supply and installation. Refer to VicRoads design note RDN-06-10 for a list of accepted products. Posts complying with AS1742.2 will also be accepted.
 13. For typical cross sections refer series 1100.
 14. For road cross sections refer series 9000.
 15. For signage schedule refer to series 4900.

Warning
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DESIGN

19/11/2021 12:18:37 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55
 Height Datum: AHD

Jacobs
 Tasmanian Government

DESIGNED: Out of Scope
 REVIEWED:

Department of State Growth
 MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 24

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4824	PRINTED DATE 19/11/2021	SHEET No. 4824
REGISTRATION NUMBER A0087.059			REVISION 0



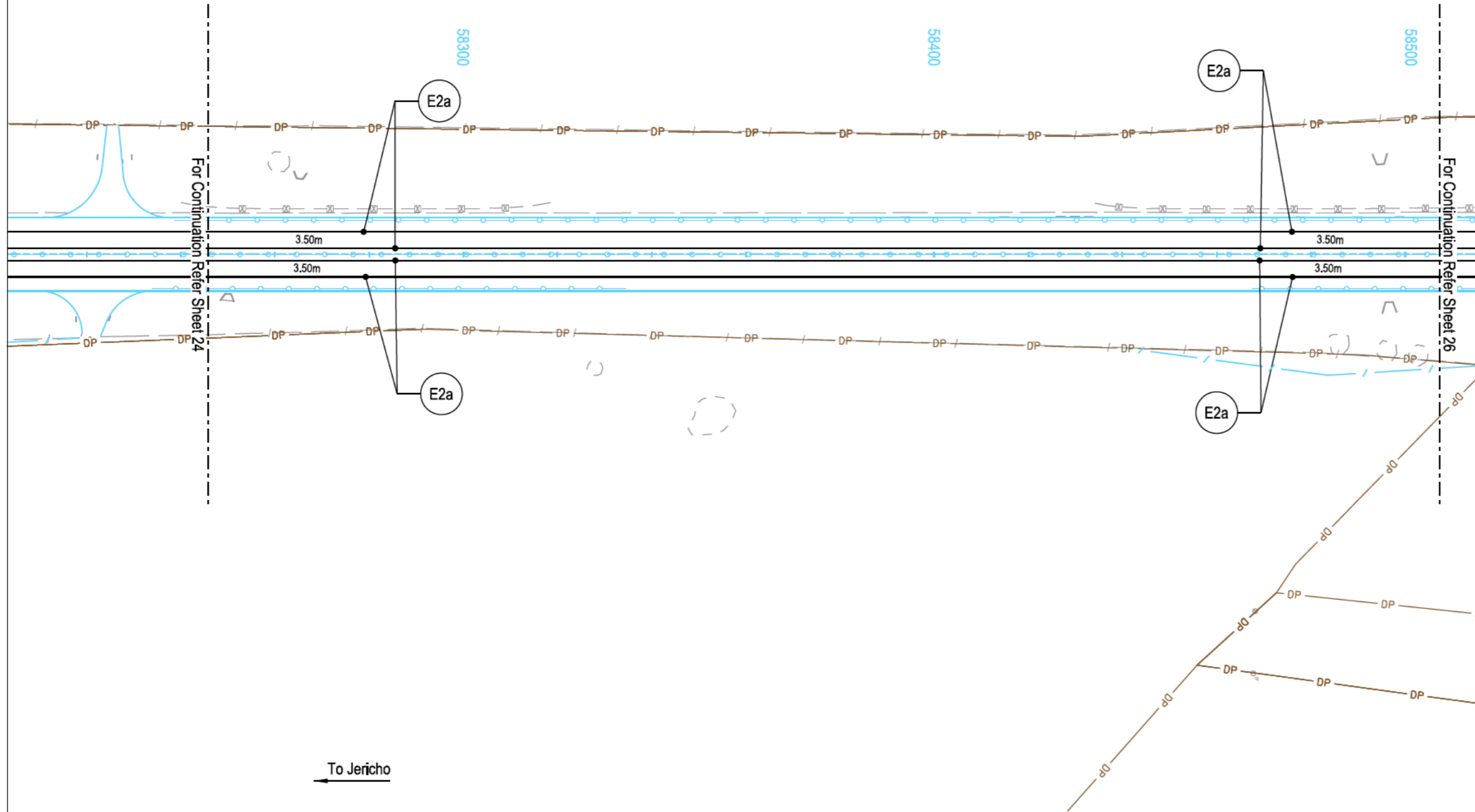
To York Plains →

← To Jericho

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
 - ↙ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
 - Functional Design
 - Proposed Fence line
 - DP Property boundary

- Notes**
1. The contractor shall locate all existing and relocated services prior to any construction related to traffic sign supports and footings.
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 - 5.3. Chevrons are to be waterborne paint.
 - 5.4. Audio - tactile marking shall be 'Type B' for both asphalt & sprayed seals.
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DESIGN

19/11/2021 12:18:56 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

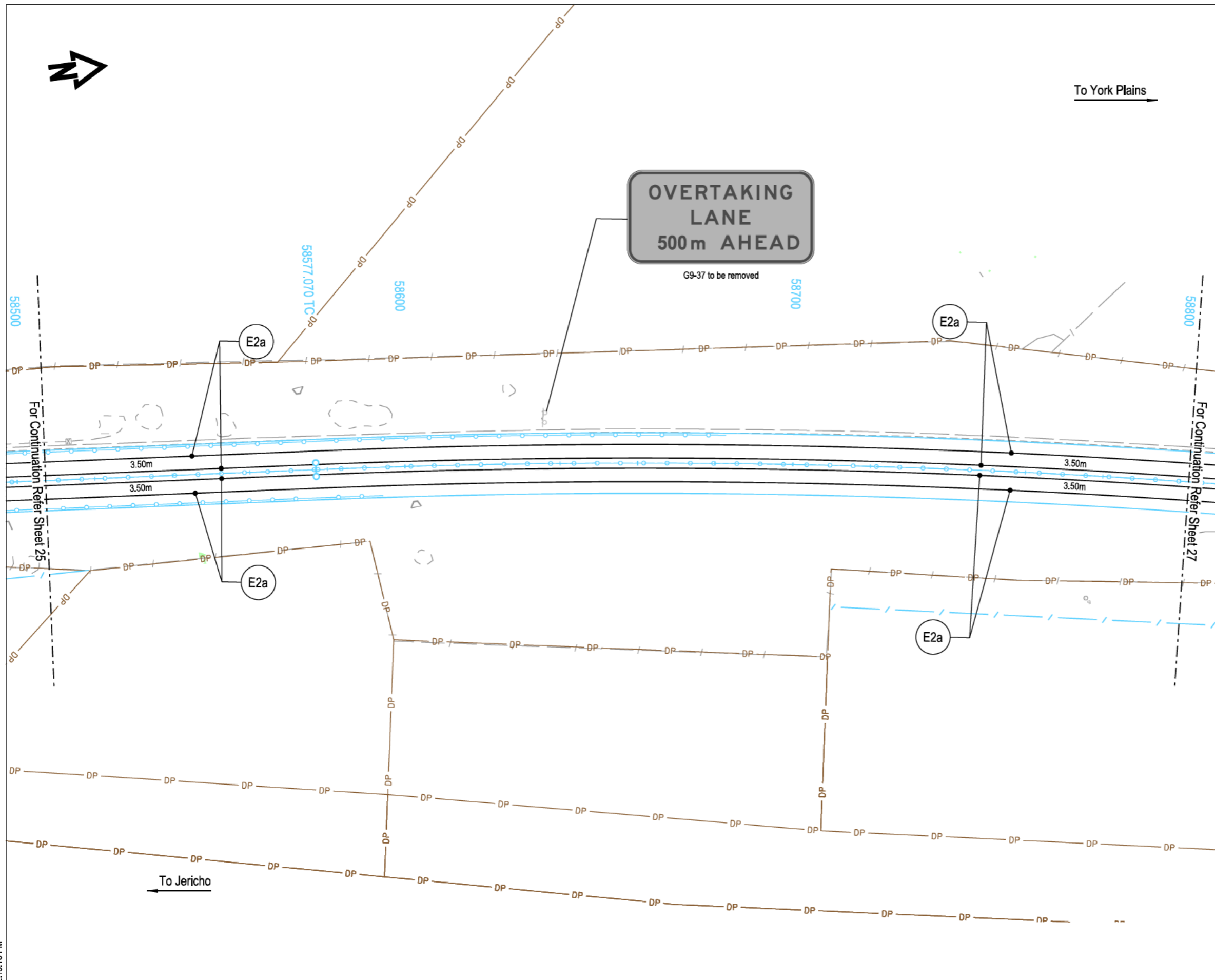
DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 25

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4825	PRINTED DATE 19/11/2021	SHEET No. 4825
REGISTRATION NUMBER A0087.059			REVISION 0



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - Edge continuity (rural) (EC2)
 - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - Holding (HL)
 - ↘ Arrow - merge (rural)
 - ↙ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
 - Functional Design
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DESIGN

19/11/2021	12:19:16 PM		
0	DESIGN ISSUE	J.E.	19/11/21
No.	Amendment Description	Initials	Date
A3 original	This sheet may be prepared using colour and may be incomplete if copied		

SCALES

0 10 20 30 40 50m

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 26

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4826	PRINTED DATE 19/11/2021	SHEET No. 4826
REGISTRATION NUMBER A0087.059			REVISION 0



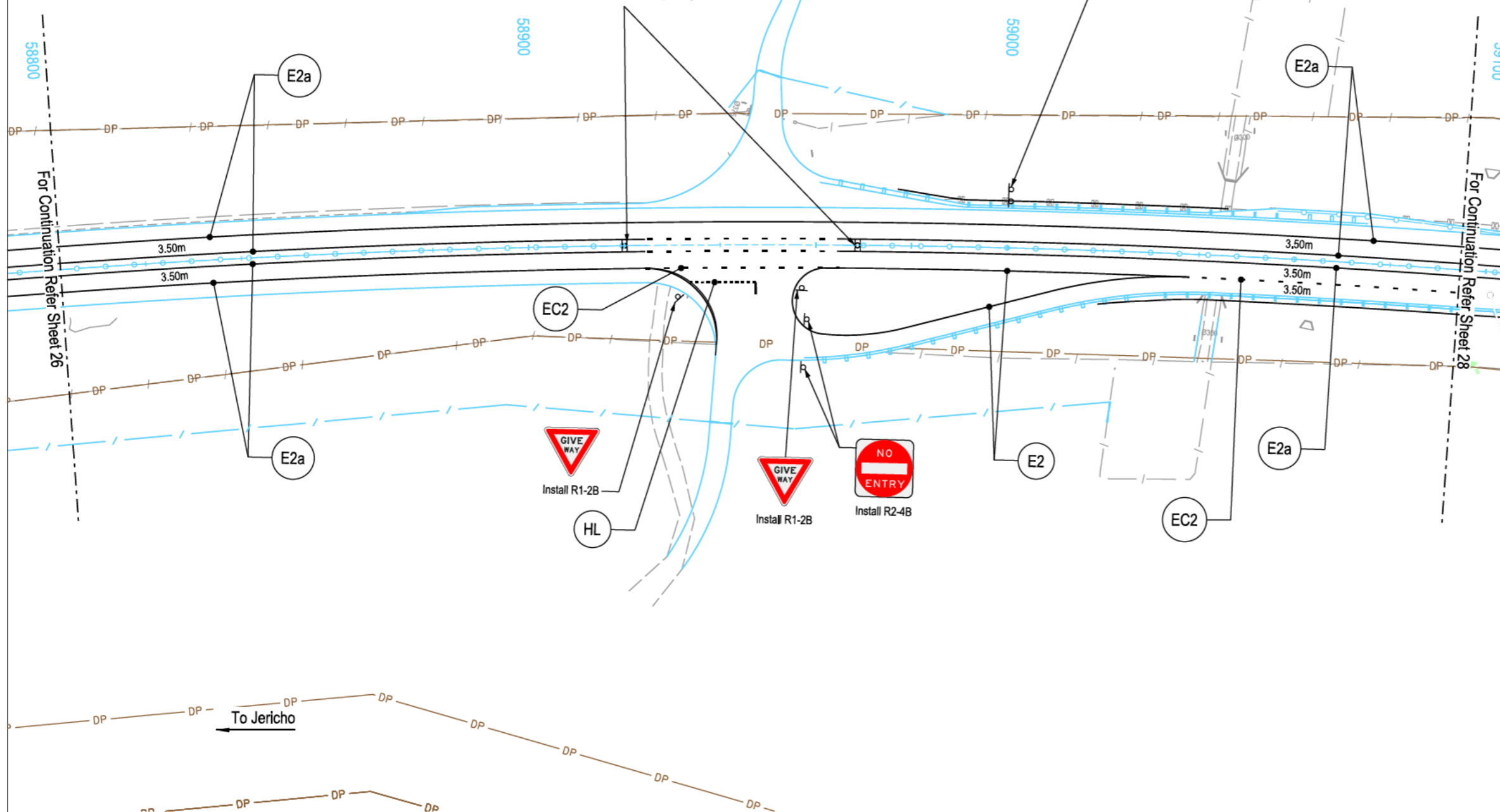
To York Plains

OVERTAKING LANE 300m AHEAD



Install R2-6B(R) & R2-3A(L)
Ensure signs are mounted not more than 750mm from road/ anchor block FSL to top of signs

Install G9-37B



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - Edge continuity (rural) (EC2)
 - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - Holding (HL)
 - ↘ Arrow - merge (rural)
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 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
 - Functional Design
 - - - Proposed Fence line
 - - - Property boundary

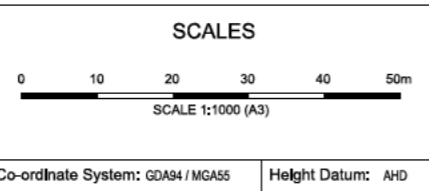
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DESIGN

19/11/2021 12:18:35 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21



Jacobs
Tasmanian Government

DESIGNED **Out of Scope**

REVIEWED

Department of State Growth
MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 27

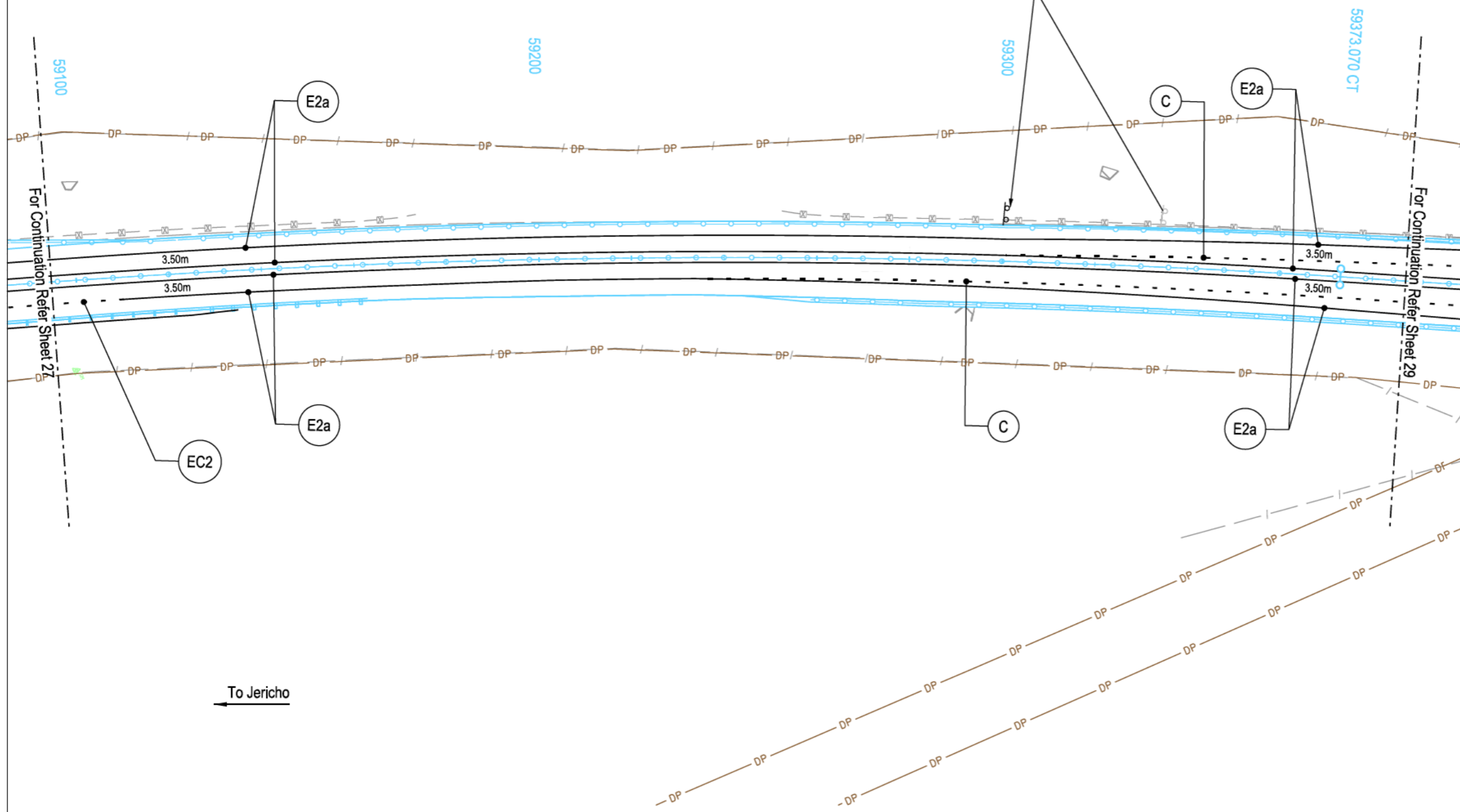
CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4827	PRINTED DATE 19/11/2021	SHEET No. 4827
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains →

**KEEP LEFT
UNLESS
OVERTAKING**

R6-29B to be relocated to new verge



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
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DESIGN

19/11/2021 12:19:56 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

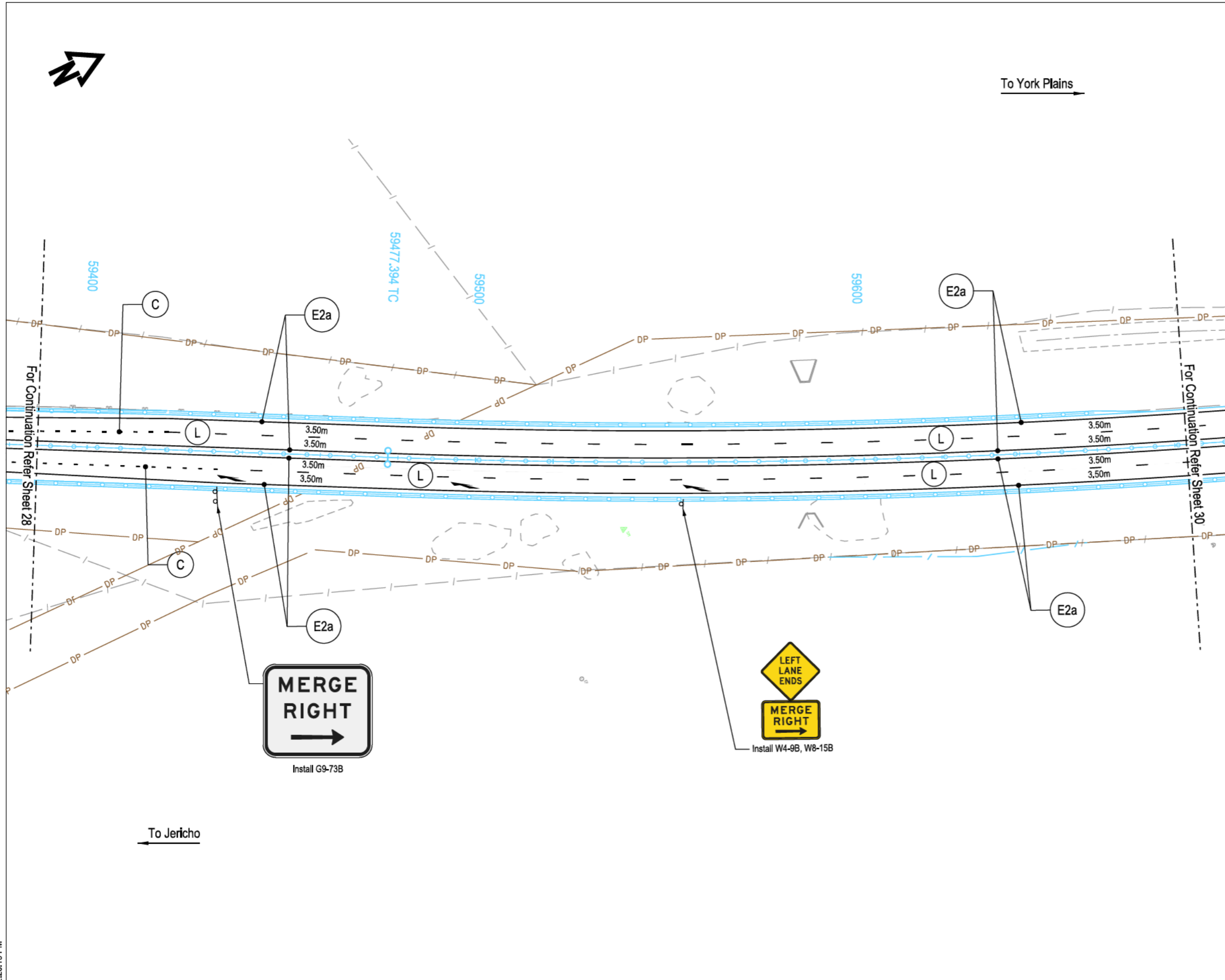
Out of Scope

DESIGNED
REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 28

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4828	PRINTED DATE 19/11/2021	SHEET No. 4828
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

- Lane (rural) (L)
- Edge rural (E2 & E2a)
- Edge continuity (rural) (EC2)
- Continuity (C)
- Barrier (both directions) (B2a)
- Continuous continuity (CC)
- Holding (HL)
- Arrow - merge (rural)
- Arrow - Turn (rural)
- Arrow - Straight Ahead
- LInemarking Identification code
- Existing sign (large)
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0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

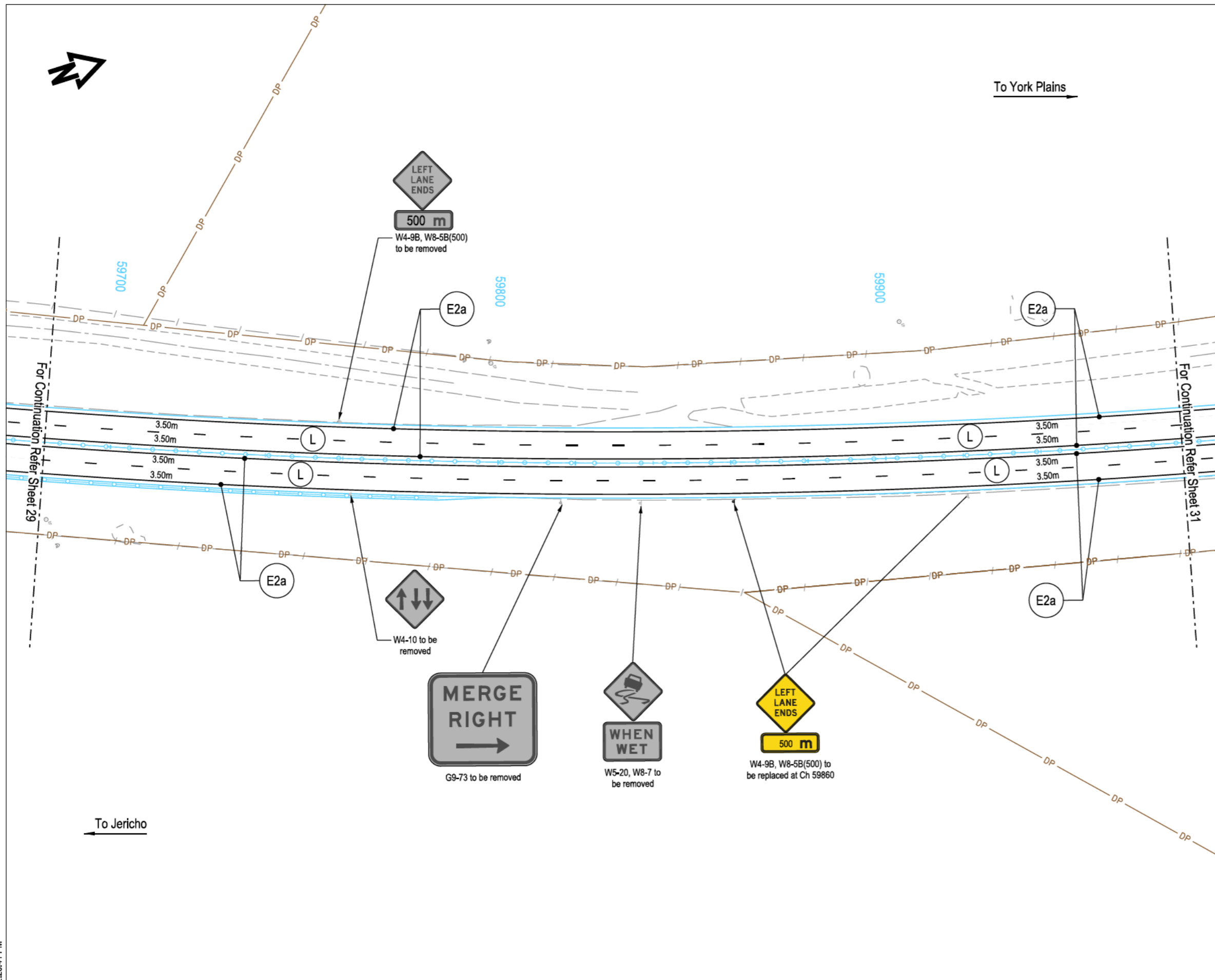
Tasmanian Government

Out of Scope

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 29

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4829	PRINTED DATE 19/11/2021	SHEET No. 4829
REGISTRATION NUMBER A0087.059			REVISION 0



Legend

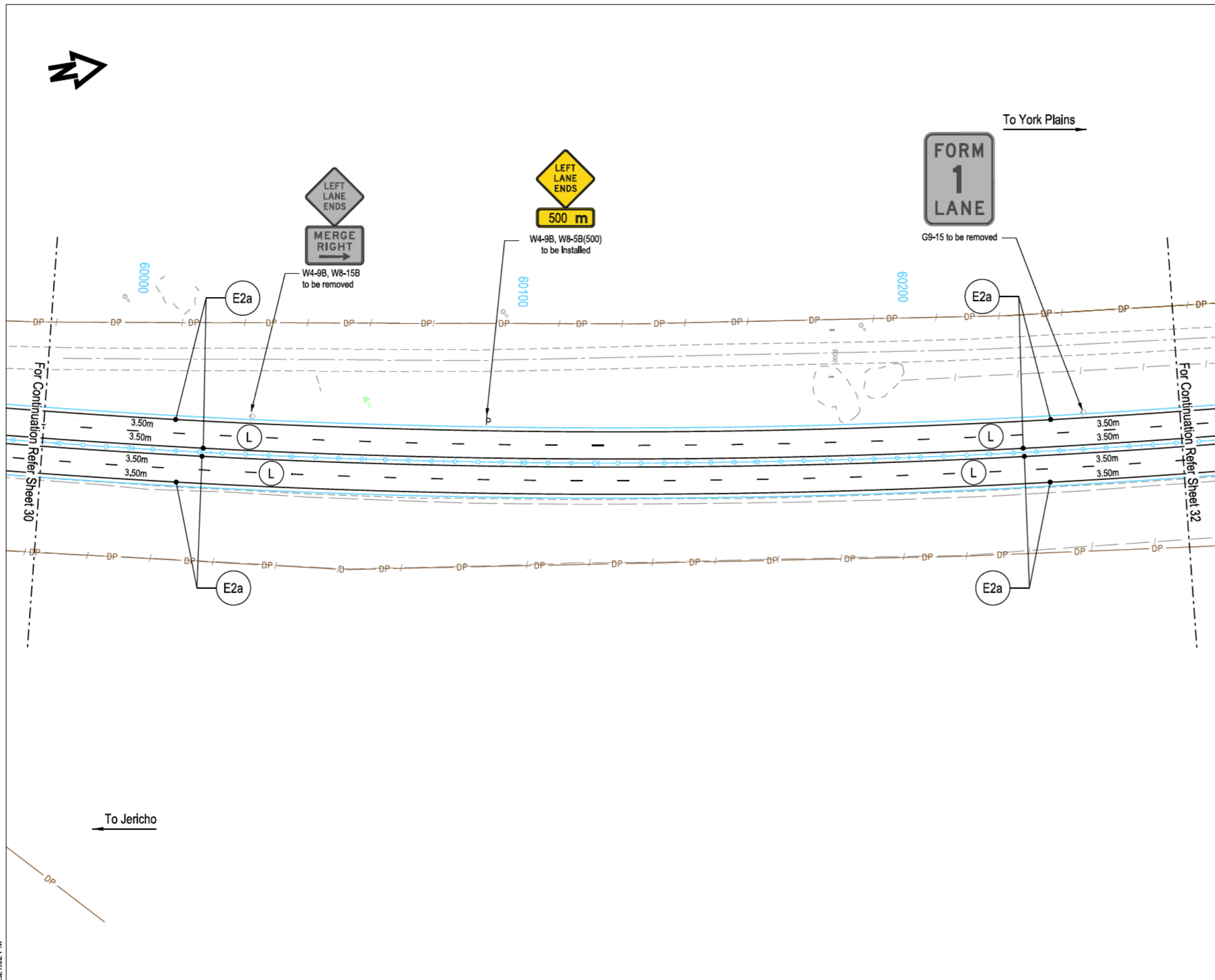
- Lane (rural) (L)
- Edge rural (E2 & E2a)
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DESIGN

	SCALES SCALE 1:1000 (A3)	 Tasmanian Government	Department of State Growth MIDLAND HIGHWAY (A0087) JERICO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE AND PAVEMENT MARKING PLAN SHEET 30	CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4830	PRINTED DATE 19/11/2021	SHEET No. 4830
0 DESIGN ISSUE No. Amendment Description Initials Date	J.E. 19/11/21	DESIGNED REVIEWED	Out of Scope	REGISTRATION NUMBER A0087.059		REVISION 0	



Legend	
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—	Edge rural (E2 & E2a)
- - - - -	Edge continuity (rural) (EC2)
- - - - -	Continuity (C)
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—	Continuous continuity (CC)
- - - - -	Holding (HL)
↘	Arrow - merge (rural)
↘	Arrow - Turn (rural)
→	Arrow - Straight Ahead
⊙ E2 ⊙ EC2	Linemarking identification code
⊙	Existing sign (large)
⊙	Existing sign (single)
⊙	Proposed sign (large)
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—	Proposed Fence line
DP	Property boundary

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DESIGN

19/11/2021 12:21:02 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

0 10 20 30 40 50m

SCALE 1:1000 (A3)

Jacobs

Tasmanian Government

Out of Scope

DESIGNED

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 31

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4831	PRINTED DATE 19/11/2021	SHEET No. 4831
REGISTRATION NUMBER A0087.059			REVISION 0



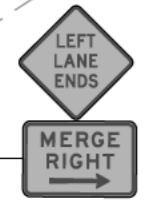
To York Plains →



W4-9B, W8-5B (500) to be installed



Remove W4-10



W4-9B and W8-15 to be removed

- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↗ Arrow - merge (rural)
 - ↘ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking Identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
 - Functional Design
 - - - - Proposed Fence line
 - - - - Property boundary

- Notes**
1. The contractor shall locate all existing and relocated services prior to any construction related to traffic sign supports and footings.
 2. Align new pavement marking with existing pavement markings at limits of work / widening.
 3. Redundant pavement markings and RRPM's are to be removed to leave a clean, undamaged pavement with a surface texture, reflective characteristics and colour comparable to adjacent pavement surface.
 4. Pavement markings to D.S.G. standard drawings SD-81.001, 002, 004 and SD-84.011
 5. Pavement markings are to be:
 - 5.1. Thermoplastic on asphalt wearing surfaces U.N.O.
 - 5.2. Directional arrows are to be thermoplastic or cold applied plastic regardless of wearing surface.
 - 5.3. Chevrons are to be waterborne paint.
 - 5.4. Audio - tactile marking shall be 'Type B' for both asphalt & sprayed seals.
 - 5.5. Straight pavement arrows used for defining two-way traffic shall be 8m in length.
 6. Arrow spacing as per DSG standard drawing SD-84.015 and SD-84.018 unless noted otherwise.
 7. Provide RRPM's and guide posts in accordance with D.S.G. standard drawings and the Specification.
 8. All existing signs are to be removed or relocated as denoted on drawings unless noted otherwise.
 9. New signs to be installed and existing signs to be reinstated as per D.S.G. standard drawings 3403-9/P22-4, 3403-9/P27-1, & 3403-9/P28-2.
 10. New guide sign faces shall be manufactured in accordance with the design provided.
 11. Signs located in medians and traffic islands to be installed using 'loc-socket' or similar approved and fitted with anti theft bolts.
 12. Refer to D.S.G. Specification Section 709 for guide post supply and installation. Refer to VicRoads design note RDN-06-10 for a list of accepted products. Posts complying with AS1742.2 will also be accepted.
 13. For typical cross sections refer series 1100.
 14. For road cross sections refer series 9000.
 15. For signage schedule refer to series 4900.

Warning
Beware of Underground Services
The location of underground services are approximate only and the exact position should be proven on site. No guarantee is given that all services are shown on these drawings.

DESIGN

19/11/2021 12:21:19 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED Out of Scope

REVIEWED [Redacted]

Department of State Growth

MIDLAND HIGHWAY (A0087)
JERICO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 32

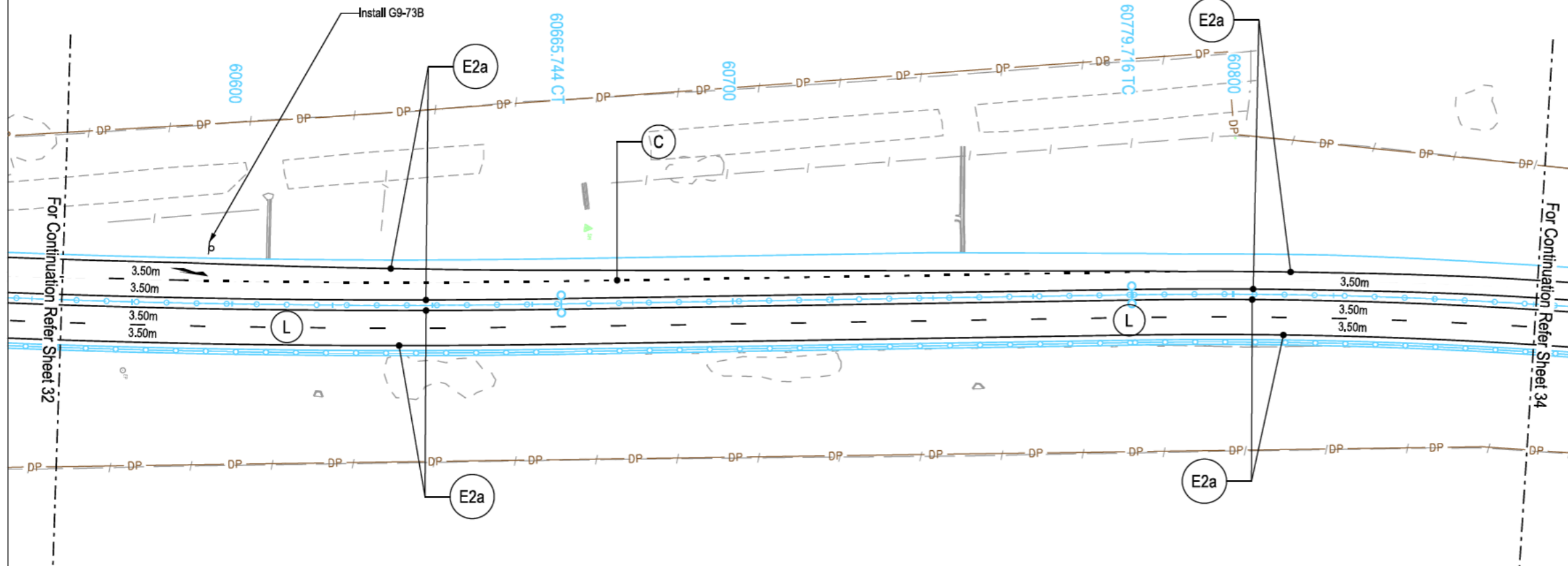
CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4832	PRINTED DATE 19/11/2021	SHEET No. 4832
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains



Install G9-73B



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - - - - Holding (HL)
 - ↘ Arrow - merge (rural)
 - ↙ Arrow - Turn (rural)
 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking identification code
 - Existing sign (large)
 - Existing sign (single)
 - Proposed sign (large)
 - Proposed sign (single)
 - ⊞ Proposed sign (double)
 - Functional Design
 - Proposed Fence line
 - DP Property boundary

- Notes**
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 - 5.5. Straight pavement arrows used for defining two-way traffic shall be 8m in length.
 6. Arrow spacing as per DSG standard drawing SD-84.015 and SD-84.018 unless noted otherwise.
 7. Provide RRP's and guide posts in accordance with D.S.G. standard drawings and the Specification.
 8. All existing signs are to be removed or relocated as denoted on drawings unless noted otherwise.
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DESIGN

19/11/2021 12:21:37 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Jacobs

Tasmanian Government

Out of Scope

Department of State Growth

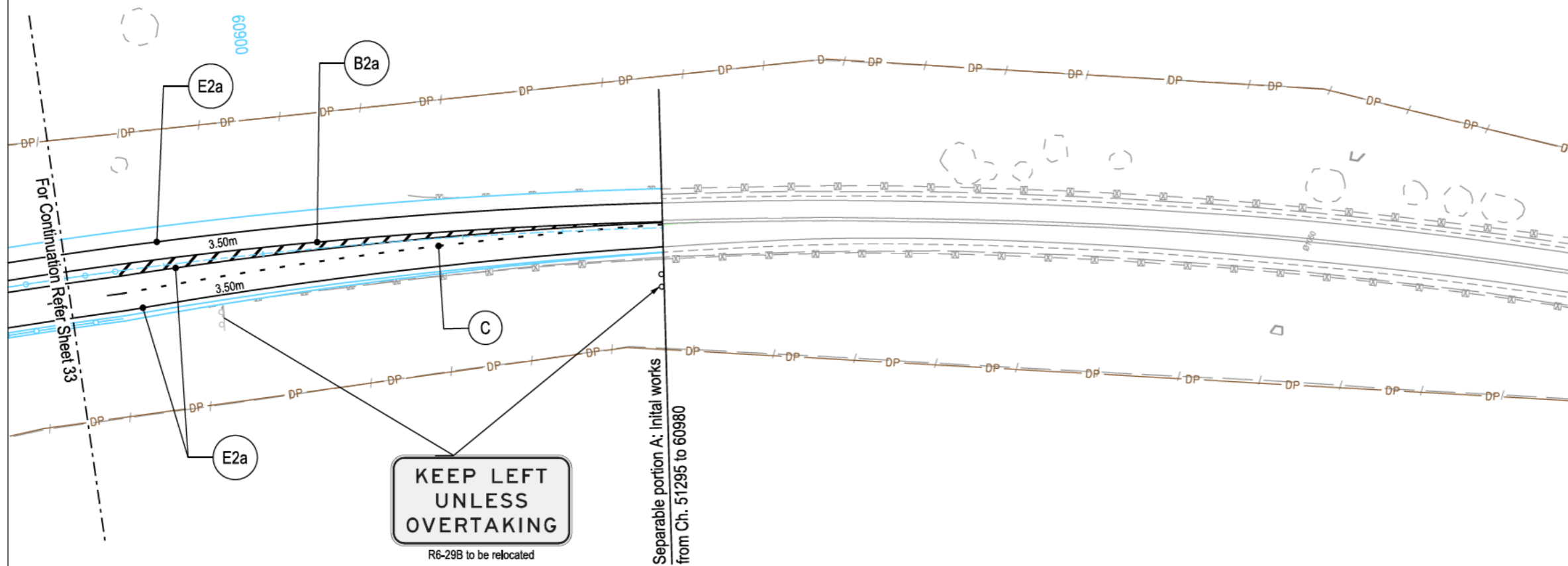
MIDLAND HIGHWAY (A0087)
JERICHO TO YORK PLAINS
ROAD IMPROVEMENTS AND SAFETY UPGRADE
SIGNAGE AND PAVEMENT MARKING PLAN
SHEET 33

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4833	PRINTED DATE 19/11/2021	SHEET No. 4833
REGISTRATION NUMBER A0087.059			REVISION 0



To York Plains

To Jericho



- Legend**
- Lane (rural) (L)
 - Edge rural (E2 & E2a)
 - - - - Edge continuity (rural) (EC2)
 - - - - Continuity (C)
 - Barrier (both directions) (B2a)
 - Continuous continuity (CC)
 - Holding (HL)
 - ↘ Arrow - merge (rural)
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 - Arrow - Straight Ahead
 - ⊙ E2 ⊙ EC2 LInemarking identification code
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DESIGN

19/11/2021 12:21:56 PM

No.	Amendment Description	Initials	Date
0	DESIGN ISSUE	J.E.	19/11/21

SCALES

SCALE 1:1000 (A3)

Co-ordinate System: GDA94 / MGA55 Height Datum: AHD

Jacobs

DESIGNED Out of Scope

REVIEWED

Department of State Growth

MIDLAND HIGHWAY (A0087)
 JERICO TO YORK PLAINS
 ROAD IMPROVEMENTS AND SAFETY UPGRADE
 SIGNAGE AND PAVEMENT MARKING PLAN
 SHEET 34

CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4834	PRINTED DATE 19/11/2021	SHEET No. 4834
REGISTRATION NUMBER A0087.059			REVISION 0

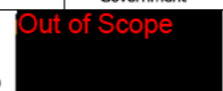
DRAWING NO.	CONTROL LINE	CHAINAGE (FROM)	CHAINAGE (TO)	SIGN					SIZE (h x w)	MOUNTING HEIGHT	PCST		POST FOOTING		STATE GROWTH STANDARD DRAWING	COMMENTS
				No. OF SIGNS PER ASSEMBLY	SIGN CODE	SIGN TYPE	DESCRIPTION	No. AND TYPE OF POST			HOLE DEPTH (mm)	DIAMETER (mm) min.				
IS262319-CAT-4000-CR-DRG-4801	MC01	-	51190	1	CUSTOM	-	MIDLAND HIGHWAY UPGRADES JERICHO TO YORK PLAINS	1600x3000	2000	2x 80 mm STEEL	900	400	Federal Naton Building Program Signage Guidelines and Drawings & 3403-9/P22-4	TO BE INSTALLED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4801	MC01	51300	51300	1	G10-3	GUIDE	L 130	450x450	1500	1x 50 mm STEEL	750	400	3403-9/P22-4 & 3403-9/P28-2	TO BE RELOCATED		
IS262319-CAT-3000-CR-DRG-4801	MC01	51310	51310	1	R4-1B(110)	REGULATORY	110 KM/H	800x600	2000	1x 50 mm STEEL	750	400	3403-9/P22-4 & 3403-9/P28-2	TO BE REPLACED		
IS262319-CAT-4000-CR-DRG-4801	MC01	51320	-	3	R2-3B(L)	REGULATORY	KEEP LEFT	800x600						TO BE REMOVED		
					D4-1-2B	GUIDE	UNIDIRECTIONAL HAZARD MARKER	600x600								
					R2-5B	REGULATORY	NO U TURN	600x600								
IS262319-CAT-4000-CR-DRG-4801	MC01	51340	-	3	W4-6B	WARNING	END DIVIDED ROAD	900x900						TO BE REMOVED		
					R2-4	REGULATORY	NO ENTRY	600x600								
					GE9-15	GUIDE	WRONG WAY	400x600								
IS262319-CAT-4000-CR-DRG-4801	MC01	-	51350	1	R6-29B	REGULATORY	KEEP LEFT UNLESS OVERTAKING	900x1800	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE INSTALLED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4801	MC01	51450	51450	2	G2-5	DIRECTIONAL	JERICHO JCN 400 M	1350x1700	2000	2x 80 mm STEEL	900	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
					G2-5	DIRECTIONAL	ELLESMERE RD JCN 400 M	1100x1700								
IS262319-CAT-4000-CR-DRG-4801	MC01	51460	51460	1	CUSTOM	-	FATIGUE CAN BE FATAL	3000x1700	2000	2x 80 mm STEEL	900	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4802	MC01	51600	51600	1	W2-1C	WARNING	CROSS ROAD AHEAD	900x900	2000	1x 60 mm STEEL	750	400		SIGN TO BE REPLACED WITH CURRENT AS1743 VERSION, SEE NOTE 1		
					R2-5B	REGULATORY	NO U TURN	600x600								
					D4-1-2B	GUIDE	UNIDIRECTIONAL HAZARD MARKER	600x600	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-3B(L)	REGULATORY	KEEP LEFT	600x400								
IS262319-CAT-4000-CR-DRG-4802	MC01	-	51700	1	W4-6B	WARNING	END DIVIDED ROAD	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4802	LOWER MARSHES RD	-	210	1	W3-2B	WARNING	GIVE WAY SIGN AHEAD	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4802	MC01	51790	51790	2	G2-1	DIRECTIONAL	FINGERBOARD SIGN	750x3500	2000	1x 50 mm STEEL	750	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED (Back to Back)		
					G2-1	DIRECTIONAL	FINGERBOARD SIGN	750x3500								
IS262319-CAT-4000-CR-DRG-4802	MC01	51810	51810	1	R1-2B	REGULATORY	GIVE WAY	900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE RELOCATED		
IS262319-CAT-4000-CR-DRG-4802	MC01	51820	51820	1	R1-2B	REGULATORY	GIVE WAY	900					3403-9/P27-1 & 3403-9/P28-2	TO BE RELOCATED		
IS262319-CAT-4000-CR-DRG-4802	MC01	51840	51840	2	G2-1	DIRECTIONAL	FINGERBOARD SIGN	1000x2600	2000	2x 65 mm STEEL*	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
					G2-1	DIRECTIONAL	FINGERBOARD SIGN	1000x2350								
IS262319-CAT-4000-CR-DRG-4803	MC01	51970	-	1	G9-37	GUIDE	OVERTAKING LANE 500 M AHEAD	1300x2600						TO REMAIN		
					W5-20	WARNING	SLIPPERY	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED		
					W6-7B	WARNING	WHEN WET	500x750								
IS262319-CAT-4000-CR-DRG-4803	MC01	52060	52060	1	W2-1C	WARNING	CROSS ROAD AHEAD	900x900	2000	1x 50 mm STEEL	750	400		SIGN TO BE REPLACED WITH CURRENT AS1743 VERSION		
IS262319-CAT-4000-CR-DRG-4804	MC01	-	52200	1	W4-11C	WARNING	TWO WAY TRAFFIC	900x900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4804	MC01	-	52200	1	W4-11C	WARNING	TWO WAY TRAFFIC	900x900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4804	MC01	52250	-	2	G2-5	DIRECTIONAL	ELLESMERE RD JCN 400 M	1100x1700						TO REMAIN		
					G2-5	DIRECTIONAL	JERICHO JCN 400 M	1350x1700								
IS262319-CAT-4000-CR-DRG-4804	MC01	52260	-	1	G6-2	GUIDE	JORDAN RIVER	410x1350						TO REMAIN		
IS262319-CAT-4000-CR-DRG-4804	MC01	52320	-	1	G6-2	GUIDE	JORDAN RIVER	410x1350						TO REMAIN		
IS262319-CAT-4000-CR-DRG-4805	MC01	-	52440	1	W4-11C	WARNING	TWO WAY TRAFFIC	900x900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4805	MC01	-	52440	1	W4-11C	WARNING	TWO WAY TRAFFIC	900x900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4805	MC01	-	52520	1	W4-6B	WARNING	END DIVIDED ROAD	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-CAT-4000-CR-DRG-4805	MC01	-	52520	2	R2-3B(L)	REGULATORY	KEEP LEFT	800x600	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					D4-1-2B	GUIDE	UNIDIRECTIONAL HAZARD MARKER	600x600								
IS262319-CAT-4000-CR-DRG-4807	MC01	53090	53090	1	G2-5	DIRECTIONAL	B31 RICHMOND JCN 800 M	1850x1650	2000	1x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4808	MC01	53410	53410	1	GE4-1	DIRECTIONAL	REASSURANCE DIRECTION SIGN	1300x2100	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4808	MC01	53450	-	1	CUSTOM	-	MIDLAND HWY UPGRADES MJD WALLS RD JUNCTION	1200x900						TO BE REMOVED		
IS262319-CAT-4000-CR-DRG-4808	MC01	53500	53500	1	R6-10-2 (Modified)	REGULATORY	25T GROSS LOAD LIMIT ON SIDE ROAD	3600x1700	2000	2x 90 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4809	MC01	53550	53550	1	G2-5	DIRECTIONAL	COAL RIVER VALLEY WINE REGION JCN 300 M	1700x1700	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4809	MC01	53620	53620	1	G1-1	DIRECTIONAL	REASSURANCE DIRECTION SIGN	2050x3000	2000	2x 90 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-CAT-4000-CR-DRG-4809	MC01	-	53630	2	D4-1-2B	GUIDE	UNIDIRECTIONAL HAZARD MARKER	600x600	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-3B(L)	REGULATORY	KEEP LEFT	800x600								
IS262319-CAT-4000-CR-DRG-4809	MC01	53790	-	1	R1-2B	REGULATORY	GIVE WAY	900						TO REMAIN		
					R1-2B	REGULATORY	GIVE WAY	900								
IS262319-CAT-4000-CR-DRG-4809	MC01	53800	-	2	R2-3B(L)	REGULATORY	KEEP LEFT	800x600						TO REMAIN		
					G2-1	DIRECTIONAL	FINGERBOARD SIGNS (x2, Back to Back)	1000x2950								
					G2-1 (L)	DIRECTIONAL	FINGERBOARD SIGN	1000x2800								
					G2-1 (R)	DIRECTIONAL	FINGERBOARD SIGN	1000x3050								
IS262319-CAT-4000-CR-DRG-4809	MC01	53800	-	5	D-4-2-3	GUIDE	BIDIRECTIONAL HAZARD MARKER	800x3200						TO BE RELOCATED		

Notes:
1. All new and relocated posts shall be frangible U.N.O. Refer RDN 06-09 for accepted frangible post systems.

DESIGN

12:22:10 PM

19/11/2021

SCALES		 		Department of State Growth MIDLAND HIGHWAY (A0087) JERICHO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE SCHEDULE SHEET 1			CONTRACT No. 3169	DRAWING NO. IA250204-OAT-4000-CR-DRG-4901	PRINTED DATE 19/11/2021	SHEET No. 4901
DESIGNED	J.E. 19/11/21				REGISTRATION NUMBER A0087.059			REVISION 0		
REVIEWED	Initials Date		Co-ordinate System: GDA94 / MGA55		Height Datum: AHD					

DRAWING NO.	CONTROL LINE	CHAINAGE (FROM)	CHAINAGE (TO)	SIGN					SIZE (h x w)	MOUNTING HEIGHT	PCST		POST FOOTING		STATE GROWTH STANDARD DRAWING	COMMENTS
				No. OF SIGNS PER ASSEMBLY	SIGN CODE	SIGN TYPE	DESCRIPTION	No. AND TYPE OF POST			HOLE DEPTH (mm)	DIAMETER (mm) min.				
IS262319-OAT-4000-CR-DRG-4809	MC01	53820	-	1	D4-2-2B	GUIDE	BIDIRECTIONAL HAZARD MARKER	1000x400							TO REMAIN	
IS262319-OAT-4000-CR-DRG-4809	MC01	53820	-	1	R1-2	REGULATORY	GIVE WAY	900							TO REMAIN	
IS262319-OAT-4000-CR-DRG-4810	MC01	54010	-	1	G1-1	DIRECTIONAL	REASSURANCE DIRECTION SIGN	2050x3000							TO REMAIN	
IS262319-OAT-4000-CR-DRG-4810	MC01	-	54020	1	R6-29B	REGULATORY	KEEP LEFT UNLESS OVERTAKING	900x1800	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE INSTALLED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4810	MC01	-	53860	2	R2-3B(L)	REGULATORY	KEEP LEFT	800x600	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					D4-1-2B	GUIDE	UNIDIRECTIONAL HAZARD MARKER	600x600								
IS262319-OAT-4000-CR-DRG-4810	MC01	54100	54100	1	CUSTOM	-	COAL RIVER VALLEY WINE REGION JCN 300 M	1700x1700	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4810	MC01	54120	54090	2	G4-1	DIRECTIONAL	REASSURANCE DISTANCE SIGN	1600x2300	2000	2x 80 mm STEEL	900	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
					G4-1	DIRECTIONAL	OATLANDS	750x2300								
IS262319-OAT-4000-CR-DRG-4810	MC01	54140	54160	1	R4-1B(110)	REGULATORY	110 KM/H	1200x900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE RELOCATED		
IS262319-OAT-4000-CR-DRG-4811	MC01	54170	-	1	CUSTOM	-	MIDLAND HWY UPGRADES MUDWALLS RD JUNCTION	1200x900						TO BE REMOVED		
IS262319-OAT-4000-CR-DRG-4811	MC01	54230	54230	1	R6-10-2 (Modified)	REGULATORY	25T GROSS LOAD LIMIT ON SIDE ROAD	3600x1700	2000	1x 90 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4811	MC01	54710	54320	1	G9-38	GUIDE	OVERTAKING LANE 3 KM AHEAD	1300x2600	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4812	MC01	54560	54560	1	G2-5	DIRECTIONAL	B31 RICHMOND JCN 800 M	1850x1650	2000	2x 65 mm STEEL	800	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4814	MC01	-	55050	2	W4-9B	WARNING	LEFT LANE ENDS	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
					W8-5B(500)	WARNING	500 M	500x750								
IS262319-OAT-4000-CR-DRG-4815	MC01	-	55320	2	W4-9B	WARNING	LEFT LANE ENDS	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
					W8-15B	WARNING	MERGE RIGHT	500x750								
IS262319-OAT-4000-CR-DRG-4815	MC01	-	55560	1	G9-73B	GUIDE	MERGE RIGHT	1200x1200	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4816	MC01	55740	55740	1	G2-5	DIRECTIONAL	JERICHO RD JCN 400 M	1050x1700	2000	2x 50 mm STEEL	750	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED		
IS262319-OAT-4000-CR-DRG-4817	MC01	56010	56010	2	W5-20	WARNING	SLIPPERY	1200x1500	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
					CUSTOM	WARNING	CAUTION ICE	1200x1500								
IS262319-OAT-4000-CR-DRG-4817	JERICHO RD	-	210	1	W8-2B	WARNING	GIVE WAY SIGN AHEAD	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4817	MC01	-	56150	2	R2-5B	REGULATORY	NO U TURN	600x600	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-3A(L)	REGULATORY	KEEP LEFT	600x450								
IS262319-OAT-4000-CR-DRG-4817	MC01	56160	56160	1	G3-5	GUIDE	JERICHO ROAD	200x1500	2000	2x 50 mm STEEL	750	400	3403-9/P23-4 & 3403-9/P27-1	TO BE REPLACED		
IS262319-OAT-4000-CR-DRG-4817	MC01	-	56170	1	R1-2B	REGULATORY	GIVE WAY	900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4817	MC01	-	56170	2	R2-3A(L)	REGULATORY	KEEP LEFT	600x450	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-5B	REGULATORY	NO U TURN	600x600								
IS262319-OAT-4000-CR-DRG-4818	MC01	56280	56280	1	G2-5	DIRECTIONAL	C 314 STONOR (TUNNACK) JCN 400 M	1600x1700	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4819	MC01	56540	56540	1	G2-5	DIRECTIONAL	JERICHO RD JCN 400 M	1050x1700	2000	2x 50 mm STEEL	750	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED		
IS262319-OAT-4000-CR-DRG-4819	MC01	56670	56670	1	R1-2B	REGULATORY	GIVE WAY	900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE RELOCATED		
IS262319-OAT-4000-CR-DRG-4819	MC01	-	56670	2	R2-3A(L)	REGULATORY	KEEP LEFT	600x450	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-5B	REGULATORY	NO U TURN	600x600								
IS262319-OAT-4000-CR-DRG-4819	MC01	56680	56680	5	G2-1	DIRECTIONAL	FINGERBOARD SIGNS (x2, Back to Back)	1000x2950	2000	4x 90 mm STEEL	1200	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
					G2-1 (L)	DIRECTIONAL	FINGERBOARD SIGN	1000x2800								
					G2-1 (R)	DIRECTIONAL	FINGERBOARD SIGN	1000x3050								
					D-4-2-3	GUIDE	BIDIRECTIONAL HAZARD MARKER	800x3200								
IS262319-OAT-4000-CR-DRG-4819	STONOR RD	-	210	1	W8-2B	WARNING	GIVE WAY SIGN AHEAD	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4819	MC01	56720	56720	2	R2-3A(L)	REGULATORY	KEEP LEFT	600x450	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-5B	REGULATORY	NO U TURN	600x600								
IS262319-OAT-4000-CR-DRG-4821	MC01	57100	57100	1	G2-5	DIRECTIONAL	C 314 STONOR (TUNNACK) JCN 400 M	1600x1700	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4821	MC01	-	57170	1	G9-38	GUIDE	OVERTAKING LANE 5 KM AHEAD	1300x2600	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE INSTALLED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4821	MC01	57450	57300	1	G9-38	GUIDE	OVERTAKING LANE 2 KM AHEAD	1300x2600	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4826	MC01	58636	-	1	G9-37	GUIDE	OVERTAKING LANE 500 M AHEAD	1300x2600						TO BE REMOVED		
IS262319-OAT-4000-CR-DRG-4827	MC01	-	59000	1	G9-37B	GUIDE	OVERTAKING LANE 300 M AHEAD	1300x2600	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE INSTALLED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58920	2	R2-3A(L)	REGULATORY	KEEP LEFT	600x450	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-6B (R)	REGULATORY	NO RIGHT TURN	600x600								
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58920	1	R1-2B	REGULATORY	GIVE WAY	900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58960	1	R1-2B	REGULATORY	GIVE WAY	900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58960	1	R2-4B	REGULATORY	NO ENTRY	600x600	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58960	1	R2-4B	REGULATORY	NO ENTRY	600x600	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58960	2	R2-3A(L)	REGULATORY	KEEP LEFT	600x450	600 (min.)	1x 50 mm STEEL	600	400	3403-9/P25 & 3403-9/P28-2	TO BE INSTALLED		
					R2-6B (R)	REGULATORY	NO RIGHT TURN	600x600								
IS262319-OAT-4000-CR-DRG-4827	MC01	-	58970	1	R1-2B	REGULATORY	GIVE WAY	900	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
IS262319-OAT-4000-CR-DRG-4828	MC01	59330	59300	1	R6-29B	REGULATORY	KEEP LEFT UNLESS OVERTAKING	900x1800	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4829	MC01	-	59430	1	G9-73B	GUIDE	MERGE RIGHT	1200x1200	2000	1x 65 mm STEEL	1000	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED, SEE NOTE 1		
IS262319-OAT-4000-CR-DRG-4829	MC01	-	59520	2	W4-9B	WARNING	LEFT LANE ENDS	750x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED		
					W8-15B	WARNING	MERGE RIGHT	500x750								

Notes:
1. All new and relocated posts shall be frangible U.N.O. Refer RDN 06-09 for accepted frangible postsystems.

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0	DESIGN ISSUE	J.E.	19/11/21
No.	Amendment Description	Initials	Date
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Co-ordinate System: G

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Out of Scope

DESIGN

PRINTED DATE	SHEET No.
19/11/2021	4902
	REVISION 0

DRAWING NO.	CONTROL LINE	CHAINAGE (FROM)	CHAINAGE (TO)	SIGN				SIZE (h x w)	MOUNTING HEIGHT	PCST		POST FOOTING		STATE GROWTH STANDARD DRAWING	COMMENTS
				No. OF SIGNS PER ASSEMBLY	SIGN CODE	SIGN TYPE	DESCRIPTION			No. AND TYPE OF POST	HOLE DEPTH (mm)	DIAMETER (mm) min.			
IS262319-CAT-4000-CR-DRG-4830	MC01	59760	-	2	W4-9B W8-5B(500)	WARNING	LEFT LANE ENDS 500 M	750x750 250x750							TO BE REMOVED
IS262319-CAT-4000-CR-DRG-4830	MC01	59760	-	1	W4-10	WARNING	LANE ALLOCATION	900x900							TO BE REMOVED
IS262319-CAT-4000-CR-DRG-4830	MC01	59820	-	1	G9-73	WARNING	MERGE RIGHT	1200x1200							TO BE REMOVED
IS262319-CAT-4000-CR-DRG-4830	MC01	59840	-	2	W5-20 W8-7B	WARNING	SLIPPERY WHEN WET	750x750 500x750							TO BE REMOVED
IS262319-CAT-4000-CR-DRG-4830	MC01	59920	59860	2	W4-9 W8-5B(500)	WARNING	LEFT LANE ENDS 500 M	750x750 250x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE REPLACED	
IS262319-CAT-4000-CR-DRG-4831	MC01	-	60090	2	W4-9B W8-5B(500)	WARNING	LEFT LANE ENDS 500 M	750x750 250x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED	
IS262319-CAT-4000-CR-DRG-4831	MC01	60030	-	2	W4-9B W8-15B	WARNING	LEFT LANE ENDS MERGE RIGHT	750x750 500x750						TO BE REMOVED	
IS262319-CAT-4000-CR-DRG-4831	MC01	60240	-	1	G9-15	GUIDE	FORM 1 LANE	1200x900						TO BE REMOVED	
IS262319-CAT-4000-CR-DRG-4832	MC01	-	60480	2	W4-9B W8-15B	WARNING	LEFT LANE ENDS MERGE RIGHT	750x750 500x750	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED	
IS262319-CAT-4000-CR-DRG-4832	MC01	60290	-	2	W4-9B W8-15B	WARNING	LEFT LANE ENDS MERGE RIGHT	750x750 500x750						TO BE REMOVED	
IS262319-CAT-4000-CR-DRG-4833	MC01	-	60600	1	G9-73B	GUIDE	MERGE RIGHT	1200x1200	2000	1x 50 mm STEEL	750	400	3403-9/P27-1 & 3403-9/P28-2	TO BE INSTALLED	
IS262319-CAT-4000-CR-DRG-4834	MC01	60890	60980	1	R6-29B	REGULATORY	KEEP LEFT UNLESS OVERTAKING	900x1800	2000	2x 65 mm STEEL	1000	400	3403-9/P23-4 & 3403-9/P27-1	TO BE RELOCATED, SEE NOTE 1	


Notes:

1. All new and relocated posts shall be frangible U.N.O. Refer RDN 06-09 for accepted frangible post systems.

DESIGN

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<p>0 DESIGN ISSUE</p> <p>No. Amendment Description Initials Date</p> <p>A3 original This sheet may be prepared using colour and may be incomplete if copied</p>				<p>SCALES</p>		 		<p>Department of State Growth</p> <p>MIDLAND HIGHWAY (A0087) JERICHO TO YORK PLAINS ROAD IMPROVEMENTS AND SAFETY UPGRADE SIGNAGE SCHEDULE SHEET 3</p>			<p>CONTRACT No.</p> <p>3169</p>	<p>DRAWING NO.</p> <p>IA250204-OAT-4000-CR-DRG-4903</p>	<p>PRINTED DATE</p> <p>19/11/2021</p>	<p>SHEET No.</p> <p>4903</p>
						<p>DESIGNED</p> <p>REVIEWED</p>			<p>REGISTRATION NUMBER</p> <p>A0087.059</p>		<p>REVISION</p> <p>0</p>			

3. Audit findings and recommendations

3.1 Outlands

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
1	Central median width does not cover full deflection zone of flexible safety barrier. Potential for a collision with a vehicle in the opposing lane.	Consider cross-section width.	Serious / Improbable	Medium	The 2.1m is adopted as part of the DSG standard cross section from Design Guidelines for Category one Roads. Refer DSG justification report for background: https://www.transport.tas.gov.au/_data/assets/file/0017/130229/Appendix_1_-_Midland_Hwy_Crosss_Section_Justification_Report_-_Sept_14without_attachments.PDF	CLOSED
2	Shoulder width of 1 m within dual lane sections provides limited recovery room. Potential for run-off road crashes.	Consider shoulder width in dual lane sections.	Serious / Occasional	High	1m shoulders adopted on 2+1 and 2+2 sections only. The rationale being to reduce impacts associated with road widening and based on the premise of 2 lane sections have sufficient maneuverability. The existing shoulders on the highway are 1m, so the proposed 1m shoulders do not decrease the available shoulder width from the current scenario. This is subject to a Departure from the Design Guidelines for Category One Roads.	CLOSED – Pending DSG Approval of Departure from Standard
3	Shoulder width of 1 m is not suitable for cyclists. Potential for a cyclist to be struck by a vehicle if attempting to use the highway.	Consider shoulder width in dual lane sections.	Serious / Improbable	Medium	As per response to Item 2, the 1m shoulders adopted on 2+1 and 2+2 sections only to reduce overall project impacts. The use of 1m shoulders does not decrease from the existing situation. The Midland Highway is not a known preferred cycling route and cyclist numbers on the highway are very low. No change proposed.	CLOSED
4	Wide 3 m shoulder provided in some single lane sections, inconsistent with treatment elsewhere on the highway. Potential for vehicles to attempt to use shoulder as a traffic lane. Potential for conflicts between vehicles or run-off road crashes.	Consider cross-section width.	Serious / Improbable	Medium	3m shoulder adopted in the design to allow construction efficiencies. There is some potential to look to other jurisdictions regarding how they manage these situations (in New Zealand, 3m shoulders require chevron markings to make it differentiable from the travel lane). It is also noted that VicRoads standards require 3m shoulders on major divided rural highways, so there is precedent for this treatment in other jurisdictions. Wider shoulders will also allow more room for broken down vehicles. No change in line marking currently proposed. DSG to monitor road performance and feedback and consider additional line markings in future, if warranted.	CLOSED

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
5	Approach sight distance is deficient at Lower Marshes Road and Jericho Road intersections due to existing horizontal geometry. Potential for collisions due to insufficient time to react to the presence of the junction.	Consider opportunities to correct deficient geometry to provide sight distance requirements. Where this is not possible consider providing advanced warning signage on approach to junctions. Consider providing directional signage at Jericho Road junction to assist in identifying presence of intersection.	Serious / Improbable	Medium	Advanced warning signage to be provided on side roads with deficient ASD identified.	CLOSED
6	Stopping sight distance and approach sight distance is severely deficient at Stonor Road and High Street intersections due to existing vertical geometry. Potential for collisions due to insufficient time to react to the presence of the junction.	Consider fixing the alignment to achieve sight distance compliant design. Where this is not possible consider providing advanced warning signage on approach to junctions.	Serious / Occasional	High	As per response to Item 4.	CLOSED
7	Road narrows substantially at Jordan River Bridge creating inconsistent road environment. Potential for collisions with roadside safety barrier due to lack of shoulder. Potential for head on collisions due to no central median safety barrier between Lower Marshes Road junction and north of Jordan River Bridge.	Consider widening bridge to maintain consistent cross-section and central median safety barrier.	Serious / Occasional	High	Bridge widening is outside the scope of works for the Midland Highway Final Stage project. W4-6B, W4-11C and pavement arrows provide advanced warning that road cross section narrows. Risk to be monitored and managed by DSG post construction.	CLOSED

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
8	Existing W-beam end terminals on northbound approach to Jordan River Bridge are not modern energy-absorbing terminals. Design does not propose to modify barrier or end terminals on northbound approach. Potential for serious injury if colliding head-on with end terminal.	Consider replacing end terminals with modern energy-absorbing end terminals (e.g. end terminal products accepted by VicRoads)	Serious / Improbable	Medium	Designs have been amended to include thriebeam on approach to Jordan River Bridge. As discussed and agreed with DSG, due to the unknown strength and condition of the bridge parapet, there is no guarantee that the system will perform as intend. However, it was concluded between Jacobs and DSG that the proposed thriebeam connections will provide a higher level of safety than the existing system. The existing barrier terminals will be removed from northbound approach and design now has roadside barrier connecting back to Lower Marshes/Old Mill Road.	CLOSED
9	No opportunity to pass vehicles turning left into property accesses in single lane sections. Potential for rear end collisions due to vehicles decelerating in the through lane.	Consider providing left turn treatments at higher use accesses and / or locations with deficient sight distance.	Serious / Occasional	High	High volume, large vehicle accesses have been design with a Basic Auxiliary Left (BAL) arrangement; this is a similar design context to other parts of the Midland Highway. This is expected to help mitigate the risk of rear end collisions at those locations. Low volume accesses have not been provided with specific BAL. However, all have a 2m – 3m wide shoulder on the approach.	CLOSED
10	Short channelised right turn treatment at Mud Walls Road. Potential for rear end collisions due to vehicles decelerating in through lane.	Consider extending right turn lane.	Serious / Improbable	Medium	Mudwalls Road intersection was upgraded as part of an earlier Midland Highway 10 Year Action Plan project, which included the installation of the short CHR. Widening works at Mudwalls Road was excluded from the scope of the Final Stage project works, therefor no change proposed.	CLOSED
11	Limited lighting of junctions within the audit area. Potential for collisions at junctions due to failure to react to the presence of the junction in time at night.	Consider flag lighting higher use junctions such as Mud Walls Road.	Serious / Occasional	High	Mudwalls Road intersection outside the extent of widening/upgrades for the Midland Highway Final Stage Project (some minor sealing, line marking and barrier extensions may take place). Additionally, Mudwalls Road intersection was upgraded as part of an earlier Midland Highway 10 Year Action Plan project, and that project did not include lighting of the intersection.	CLOSED
12	Property accesses located opposite junctions or at u-turn facilities (Stonor Road, turn facility at CH 58950, High Street) allow right turn entry to accesses from the through lane. Potential for rear end collisions with stationary vehicles waiting to turn right in the through lane.	Consider realigning accesses to prevent right turns.	Serious / Occasional	High	At Lemon Springs, the access is integrated at the G-Turn, which is a common treatment on the Midland Highway at turn facilities. The accesses opposite Stonor Road and High Street have remained in their current position, and sight lines have been checked to ensure SISD is achieved at these locations. These accesses are also infrequently used paddock accesses. A "No Right Turn" sign is to be mounted at the end of the WRMB to discourage right-in movements.	CLOSED

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
13	On-highway turn facility (CH 59000) is not signed and no left turn treatment is provided. Turn facility indicated by short length of broken line marking. Potential for rear end collisions due to unexpected deceleration in through lane, or sudden deceleration due to failure to detect turn facility in sufficient time.	Consider signing turn facility and providing left turn treatment.	Serious / Improbable	Medium	On highway G-Turns are not generally signed. This is because drivers other than local landowners may be confused by the operation of the turn facility, and the G-Turns are installed for landowner operations (usually farming). The broken lime marking generally extends 70-80m prior to the G-Turn to guide vehicles to begin slowing and indicating, reducing the risk of rear end crashes. The additional line marking has been included in the design resulting from feedback from other Midland Highway G-Turn facilities. No change proposed.	CLOSED
14	Potential for conflict between vehicles and pedestrians due to school bus stops on the highway at Stonor Road and Lemon Hills. Potential for school children to wait adjacent to the highway and / or cross the highway.	Consider relocating bus stops from the highway to side roads or providing warning signage on the highway.	Catastrophic / Improbable	High	A bus stop is to be provided off network on Stonor Road, which can cater for both north and southbound movements. This will be incorporated into the design prior to construction and will significantly mitigate the risk at this location. The Lemon Hills stop is not high use (and may not be used in future), south bound stop to be part of G-Turn (reducing exposure the highway), north bound to have widened shoulder (which is consistent with other low use bus stops on the Midland Highway). The proposed stops have been discussed with DSG Passenger Transport Unit.	CLOSED
15	The central median barrier requires pedestrians crossing the road to access bus stops to use long diagonal on pavement movements. Additional time on pavements for vulnerable users increases crash risk.	Consider and make allowance for potential access movements by pedestrians	Serious / Improbable	Medium	Refer to response for Item 14. Bus stop passengers not expected to be required to cross the highway following completion of works.	CLOSED
16	Existing white Jericho Road sign difficult to detect due to size. Potential for vehicle conflicts due to late recognition of the need to enter turn lanes.	Consider replacing Jericho Road sign with green directional sign consistent with other intersections.	Serious / Improbable	Medium	Jericho Road is a local road that would not usually have a green directional sign associated. A new fingerboard sign will be provided for Jericho Road, which will be consistent with other local roads on the highway. Stonor Road has advanced warning guide sign signs 400m in advance on each side of the intersection.	CLOSED

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
17	Potential for signage adjacent to accesses opposite Stonor Road and High Street to impact sight distance.	Ensure signage location does not impact sight distance at accesses.	Serious / Improbable	Medium	<p>Accesses are in same location as existing, which currently has the same signs. Limited in our ability to change signage locations significantly. The project is re-designing the accesses to a higher standard. Both accesses are paddock/farm accesses.</p> <p>Based on a review of point cloud data for the project area, the proposed location of the signs at Stonor Road should not impede sight distances, given that the accesses and signs are generally remaining in their current position. There is some potential that the sign location at the access opposite High Street could impeded sight distances – further assessment needed in the Final Design phase for the northern section of works.</p> <p>This issue should also be reviewed in a pre-opening road safety audit and if needed the signs shifted closer to the property boundary at that time.</p>	<p>CLOSED – Southern Section</p> <p>OPEN – Northern Section</p>
18	Keep left signage not provided at beginning of flexible safety barrier at CH 63080, 63580, 64500 64950. Potential for vehicles to enter the wrong carriageway.	Provide keep left signage at beginning of flexible safety barrier.	Serious / Improbable	Medium	Design to be revised to ensure keep left signs are at the beginning of the flexible safety barrier.	CLOSED
19	Signage for U-turn Bay on Interlaken Road not provided at decision point (i.e. start of left turn and right turn lanes). Potential for vehicle conflicts due to late recognition of the need to enter turn lane.	Provide U-turn Bay signage at the start of turn lanes.	Serious / Improbable	Medium	Advance signage provided on both approaches to Interlarken Road prior to left and right turn lanes, as well as signage at the intersection which is expected to be visible from the start of the turn lanes. At this stage not recommending change from current proposed locations, which are consistent with other parts of the Midland Highway. Further investigation of this issue will be undertaken in Final Design phase.	OPEN – Northern Section
20	Two locations have vertical geometry which is deficient for 110 km/h. Sight distance limited due to crest. Potential for collisions due to insufficient time to observe and react to hazards on the road.	Consider providing crest warning signage.	Serious / Improbable	Medium	One of the major project objectives was to maintain the existing highway geometry and maximise the use of the existing pavement within the upgrades. As per Section 3.4 of the Department of State Growth Design Guidelines for Category One Roads: "in the context of Bownfield sites, major changes to vertical alignments are not considered value for money...". In the development of the vertical design, any curves with a K value less than 110km/h have been investigated to see if a cost-effective mitigation can be achieved and implemented where relevant. Where not able to be cost effectively achieved, a formal Departure from Standard has been requested.	CLOSED – Pending DSG Approval of Departure from Standard

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
21	Deficient SSD in a number of locations due to central median or roadside safety barrier. Potential for collisions due to insufficient time to observe and react to hazards on the road. Combined with access at 55650, 61180, 67380 and intersection at 56700 (Stonor), 66650 (High Street).	Consider opportunities to improve SSD where possible, focusing on areas where an access or intersection occurs in combination with limited sight distance.	Serious / Improbable	Medium	Exploration of SSD improvements been made throughout the design development, however SSD deficiencies still occur within the project area due to maintaining the existing highway geometry. Any Normal Design Domain (NDD) deficiencies are documented in relevant Departure from Standards.	CLOSED – Pending DSG Approval of Departure from Standard
22	Individual power poles close to the road in several locations pose a potential hazard to errant vehicles.	Consider protecting hazard with safety barrier.	Serious / Improbable	Medium	All power poles within the clear zone for the Oatlands section are either protected through the use of a roadside barrier or relocated outside the clear zone.	CLOSED

Notes:

- Drafting error- relocation of temporary roadworks sign at CH 66760
- Roadside barrier protecting culvert at CH 53100 shown on general arrangement but missing from cross sections

3.2 Ross

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
1	Central median width does not cover full deflection zone of flexible safety barrier. Potential for a collision with a vehicle in the opposing lane.	Consider cross-section width.	Serious / Improbable	Medium	The 2.1m is adopted as part of the DSG standard cross section from Design Guidelines for Category one Roads. Refer DSG justification report for background: https://www.transport.tas.gov.au/_data/assets/file/0017/130229/Appendix_1_-_Midland_Hwy_Cross_Section_Justification_Report_-_Sept_14without_attachments.PDF	CLOSED
2	Shoulder width of 1 m within dual lane sections provides limited recovery room. Potential for run-off road crashes.	Consider shoulder width in dual lane sections.	Serious / Occasional	High	1m shoulders adopted on 2+1 and 2+2 sections only. The rationale being to reduce impacts associated with road widening and based on the premise of 2 lane sections have sufficient maneuverability. The existing shoulders on the highway are 1m, so the proposed 1m shoulders do not decrease the available shoulder width from the current scenario. This is subject to a Departure from the Design Guidelines for Category One Roads.	CLOSED – Pending DSG Approval of Departure from Standard
3	Shoulder width of 1 m is not suitable for cyclists. Potential for a cyclist to be struck by a vehicle if attempting to use the highway.	Consider shoulder width in dual lane sections.	Serious / Improbable	Medium	As per response to Item 2, the 1m shoulders adopted on 2+1 and 2+2 sections only to reduce overall project impacts. The use of 1m shoulders does not decrease from the existing situation. The Midland Highway is not a known preferred cycling route and cyclist numbers on the highway are very low. No change proposed.	CLOSED
4	Wide 3 m shoulder provided in some single lane sections, inconsistent with treatment elsewhere on the highway. Potential for vehicles to attempt to use shoulder as a traffic lane. Potential for conflicts between vehicles or run-off road crashes.	Consider cross-section width.	Serious / Improbable	Medium	3m shoulder adopted in the design to allow construction efficiencies. There is some potential to look to other jurisdictions regarding how they manage these situations (in New Zealand, 3m shoulders require chevron markings to make it differentiable from the travel lane). It is also noted that VicRoads standards require 3m shoulders on major divided rural highways, so there is precedent for this treatment in other jurisdictions. Wider shoulders will also allow more room for broken down vehicles. No change in line marking currently proposed. DSG to monitor road performance and feedback and consider additional line markings in future, if warranted.	CLOSED

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
5	Approach sight distance is deficient at Ashby Road and Chiswick Road intersections due to existing horizontal geometry. Potential for collisions due to insufficient time to react to the presence of the junction.	Consider opportunities to correct deficient geometry to provide sight distance requirements. Where this is not possible consider providing warning signage on approach to junctions.	Serious / Improbable	Medium	Advanced warning signage to be provided on all side roads with deficient ASD.	CLOSED
6	No opportunity to pass vehicles turning left into property accesses in single lane sections. Potential for rear end collisions due to vehicles decelerating in the through lane.	Consider providing left turn treatments at higher use accesses.	Serious / Occasional	High	<p>High volume, large vehicle accesses have been design with a Basic Auxiliary Left (BAL) arrangement; this is a similar design context to other parts of the Midland Highway. This is expected to help mitigate the risk of rear end collisions at those locations.</p> <p>Low volume accesses have not been provided with specific BAL. However, the increased shoulder width for the majority of the project site is expected to improve access movements.</p>	CLOSED
7	Limited lighting of junctions within the audit area. Potential for collisions at junctions due to failure to react to the presence of the junction in time at night.	Consider lighting higher use junctions.	Serious / Occasional	High	Scope of works has limited lighting of junctions to replacing existing lights (Roseneath and Chiswick Road).	CLOSED
8	Property access located opposite Auburn Road, allows right turns into access from the through lane. Location at the intersection suggests right turns in and out are allowed. Potential for rear end collisions with stationary vehicles waiting to turn right in the through lane	Consider realigning access to prevent right turns in or out.	Serious / Occasional	High	The access opposite Auburn Road is in its current position. The Final Design phase will investigate if this access can be closed.	OPEN – Southern Section

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
9	Road narrows substantially at Macquarie River Bridge creating inconsistent road environment. Potential for collisions with roadside safety barrier due to lack of shoulder. Potential for head on collisions due to no central median safety barrier.	Consider widening bridge to maintain consistent cross-section and central median safety barrier or provide appropriate advance warning.	Serious / Occasional	High	Bridge widening is outside the scope of works for the Midland Highway Final Stage project. W4-6B, W4-11C and pavement arrows provide advanced warning that road cross section narrows. Risk to be monitored and managed by DSG post construction.	CLOSED
10	On highway turn facilities (CH 104300, 106900) are not signed and no left turn treatment is provided. Turn facility indicated by short length of broken line marking. Potential for rear end collisions due to unexpected deceleration in through lane, or sudden deceleration due to failure to detect turn facility in sufficient time.	Consider signing turn facility and providing left turn treatment.	Serious / Improbable	Medium	On highway G-Turns are not generally signed. This is because drivers other than local landowners may be confused by the operation of the turn facility, and the G-Turns are installed for landowner operations (usually farming). The broken lime marking generally extends 70-80m prior to the G-Turn to guide vehicles to begin slowing and indicating, reducing the risk of rear end crashes. The additional line marking has been included in the design resulting from feedback from other Midland Highway G-Turn facilities. No change proposed.	CLOSED
11	Road narrows at northern end of project area with central median terminating. Potential for head on collisions due to no central median safety barrier.	Consider relocating northbound 80 km/h speed limit sign south to beginning of unseparated road section or extending central median to the change in speed limit.	Serious / Improbable	Medium	80km/h speed signs have been moved closer to the termination of the central median barrier (Ch. 111650).	CLOSED
12	Potential for signage adjacent to access opposite Auburn Road to impact sight distance.	Ensure signage location does not impact sight distance at access.	Serious / Improbable	Medium	The access opposite Auburn Road is in its current position. The Final Design phase will investigate if this access can be closed.	OPEN – Southern Section
13	Two directional signs for Ross provided just in advance of unsigned turn facility. Potential for unfamiliar drivers to mistakenly turn into turn facility.	Consider placement of directional signage.	Minor / Occasional	Medium	Placement of signs constrained. Directional signs are proposed adjacent to the G-Turn notifying drivers that the township of Ross is upcoming. Relocating signs south expected to be too close to the Ross junction to provide adequate warning to drivers, while relocating to the north could create a more likely scenario for drivers to confuse the G-Turn as the Ross junction. To be further investigated during Final Design for this section of works.	OPEN – Southern Section

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
14	Signage for U-turn Bays at CH 105050 and 110000 not provided at decision point (i.e. start of left turn and right turn lanes). Potential for vehicle conflicts due to late recognition of the need to enter turn lane.	Provide U-turn Bay signage at the start of turn lanes.	Serious / Improbable	Medium	U-Turn Bay signs have been moved to decision points at the start of turn lanes.	CLOSED
15	<i>End Divided Road</i> sign at northern end of project area provided slightly late and not followed by <i>Two-way</i> sign.	Relocate <i>End Divided Road</i> sign south and follow with <i>Two-way</i> sign, consistent with signage elsewhere within the site.	Serious / Improbable	Medium	End Divided Road sign to be moved approximately 150m south to coincide with end of median barrier. <i>Two-way</i> sign to be installed.	CLOSED
16	Two locations have horizontal curves with a design speed of 100 km/h. Potential for vehicles to lose control on curves as design speed is lower than the posted speed limit.	Consider improving geometry, providing additional shoulder width or advance advisory signs on deficient curves.	Serious / Improbable	Medium	One of the major project objectives was to maintain the existing highway geometry and maximise the use of the existing pavement within the upgrades. Significant changes to existing geometry are out of scope for this project. Both curves are in locations where the shoulders are 3m in width. The two curves are noted in a formal Departure from Standard provided to DSG.	CLOSED – Pending DSG Approval of Departure from Standard
17	Five locations have vertical geometry which is deficient for 110 km/h (4 crests, 1 sag). Sight distance limited at crests. Potential for collisions due to insufficient time to observe and react to hazards on the road.	Consider providing crest warning signage.	Serious / Improbable	Medium	Crest warning signs to are not currently installed and have not generally been implemented on the highway. Whilst some crest deficiencies will be present, overall the highway is expected to be safer with the proposed upgrades installed. Based on the wording in AS1742.2 Section 4.5 with regard to treatment of substandard vertical curves, CREST (W5-11) signage is only used where a no-overtaking zone is warranted. It is not practicable for the instances of deficient vertical crest curves on this project due to the presence of the median barrier blocking overtaking. The deficient vertical curves are noted in a formal Departure from Standard provided to DSG.	CLOSED – Pending DSG Approval of Departure from Standard

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
18	Potential for off-road turn facility at CH 105050 to be used as a school bus stop. Potential for conflict between pedestrians and vehicles using turn facility.	Provide safe location for buses to pull over / pedestrians to wait.	Serious / Improbable	Medium	Provision of school bus stops within turn facilities has been undertaken on other locations on the Midland Highway (e.g. Highland Lakes Road), and if this was to occur it would present a much safer alternative than current situation where school buses pull over on the highway. Adequate sight distance is expected to be available if a vehicle uses the U-Turn bay at the same time as a school bus. No change proposed.	CLOSED
19	Potential for conflict between vehicles and pedestrians due to school bus stops on the highway at Simpson Street. Potential for school children to wait adjacent to the highway and / or cross the highway.	Consider relocating bus stop from the highway, providing warning signage on the highway or reducing speed limit in advance of the bus stop.	Serious / Improbable	High	The speed limit will be reduced in advance of the potential bus stop location. The 80km/h speed zone will be relocated approximately 300m south of Simpson Street, which is expected to help mitigate this risk somewhat.	CLOSED
20	Deficient SSD in a number of locations due to central median or roadside safety barrier. Potential for collisions due to insufficient time to observe and react to hazards on the road. Combined with access at 100580, 103720, turn facility at 104300, 105060 and intersection at 100200 (Roseneath), 101920 (Ashby), 103950 (Chiswick), 105060.	Consider opportunities to improve SSD where possible, focusing on areas where an access or intersection occurs in combination with limited sight distance.	Serious / Improbable	Medium	Opportunities to improve SSD have been considered, though most deficiencies will remain due to the scope of the project and objective to maintain existing geometry and maximise the use of existing pavement. SSD deficiencies are subject to a formal Departure from Standard.	CLOSED – Pending DSG Approval of Departure from Standard
21	Steep existing batter slope around CH 106200 poses potential hazard to errant vehicles.	Review batter slopes and consider protecting existing steep batters with safety barrier.	Serious / Improbable	Medium	Widening and roadside works have been limited in this location to reduce impact and scope of the works. An existing dual overtaking lane section is being revised to a 2+1 lane arrangement. Whilst the existing batters are steep, they are only 1-1.5m in height and the road alignment is generally straight, therefore the risk posed could be considered relatively low. Although the existing roadside batters remain in place, the overall safety of the highway has improved through the provision of a central median barrier and wider shoulders for the northbound direction.	CLOSED

No.	Finding	Recommendation	Severity / Frequency	Risk rating	Response	Proposed Status
22	Continuous power poles close to the road between CH 97650 – 100000 and CH 107300-111900 pose a potential hazard to errant vehicles.	Consider relocating or protecting hazard with safety barrier.	Serious / Occasional	High	<p>Safety barriers are not currently provided in these locations as the updated methodology for evaluating roadside hazards in Austroads Guide to Road Design Part 6 (2020) suggests the overall risk posed by these power poles is low. This outcome is supported by the road geometry in these areas (straight), and also the lack of clear crash history attributable to these hazards. The avoidance of roadside barriers or pole relocations in these areas also helps to reduce the overall impact of the highway upgrades.</p> <p>The overall roadside risk is also decreased in some of the locations through the provision of a 2 m shoulder and median safety barrier which makes the highway 'divided' rather than 'undivided' as it currently is.</p> <p>Avoiding roadside barrier for hazards that are seemingly low risk is expected to have benefits for how roadside maintenance activities are undertaken, and will also help facilitate access to the existing power poles in case of an electrical issue.</p> <p>The Department of State Growth should monitor the locations where hazards exist within the clear zone for any trends of vehicles contacting these hazards. If necessary, the Department of State Growth may need to review the roadside hazards in future. Options may include installation of barriers on the existing batter hinge points to reduce widening required, or installation of underground power cable to remove hazards altogether (Noting that this would be a relatively costly option).</p> <p>The power pole hazards in the clear zone are noted in a formal Departure from Standard provided to DSG.</p>	CLOSED – Pending DSG Approval of Departure from Standard
23	Existing white signage at Auburn Road and Ashby Road difficult to detect due to size. Potential for vehicle conflicts due to late recognition of the need to enter turn lanes.	Consider replacing with green directional signage consistent with other intersections.	Serious / Improbable	Medium	Revised signage for Auburn and Ashby Road will be considered in the Final Design phase.	OPEN – Southern Section

From: Out of Scope
Sent: Thursday, 6 February 2025 1:05 PM
To: Out of Scope @bigpond.com
Subject: FW: Mud walls Road Tuesday 07/01/2025
Attachments: Mud Walls Road Junction 7 Jan 2025.png

Importance: High

Out of Scope

I'm not fully across the delivery of recent upgrades to this junction but I do note a right turn lane has previously been provided and wire rope median and embankment barrier has been installed in recent years.

One would expect there are overall safety benefits of these barriers, they do however reduce 'maneuvre' room in some instances at junctions/conflict zones.

A preferred arrangement to the Seagull type right turn out treatment you mention would be commencing the northbound overtaking immediately after of the junction. I'm told project or funding issues prevented this from happening.

In some cases there is also merit in greater separation between the southbound through lane and the left turn deceleration lane into Mud Walls Road.

It is difficult to consider further changes to the junction as no money is allocated and there has been recent work.

The Department will however take a closer look and continue to monitor its performance against others.

Out of Scope

Out of Scope Traffic Engineering (south) State Roads | Department of State Growth
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-----Original Message-----

From: Out of Scope <Out of Scope@stategrowth.tas.gov.au>
Sent: Tuesday, 4 February 2025 3:00 PM
To: State Roads Corporate Mail <StateRoadsCorporateMail@stategrowth.tas.gov.au>
Subject: FW: Mud walls Road Tuesday 07/01/2025
Importance: High

Good afternoon.

Please see the below correspondence received in relation to safety concerns at the junction of Mud Walls Road and Midland Highway.

As there is no active project at the junction of Mud walls and Midland Highway, can I please request this be added to a workflow (for record keeping) and issued to the Traffic Engineering Branch for action?

Happy to further discuss.

Kind regards